

HARTFORD ZONING BOARD OF ADJUSTMENT

July 24, 2024

Meeting Notice and Agenda

The Hartford Zoning Board of Adjustment will meet on Wednesday, July 24, 2024, beginning at 6:30 p.m. to consider the following Applications and Administrative Matters.

This meeting will be conducted in person at Hartford Town Hall, 171 Bridge Street, White River Junction, Vermont. In addition, you may join the meeting remotely as follows:

Microsoft Teams

[Join the meeting now](#)

Meeting ID: 241 552 196 900

Passcode: GoXaDc

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1 802-377-3677

Phone conference ID: 130 906 581#

If you cannot access this meeting notice electronically or if you are having difficulty connecting to the meeting, please email Jo-Ann Ells at jells@hartford-vt.org

A. Public Hearings, beginning at 6:30 p.m.

1. ~~Application #06-24 by 99 Depot Street, LLC (owner) and Orian Welling (applicant) for Conditional Use Approval under section 260-16 of the Hartford Zoning Regulations to convert an existing building to a multi-unit dwelling, lot 22-0253-000, 99 Depot Street, Wilder, in VR-C and R-1 zoning districts.~~
2. Application #07-24 by 12 Tremont Street, LLC (owner) and Tim Sidore (applicant) for Conditional Use Approval under section 260-16 and 260-25 of the Hartford Zoning Regulations to convert an existing structure into a multi-unit dwelling, lot 09-0072-000, 1200 Christian Street, Hartford, in a proposed Planned Development.

B. Administrative matters: 1. Public Comment. 2. Minutes. 3. Administrative Permits. 4. Discuss starting Hearings later. 5. Availability for the next Public Hearing.

Application materials are available at the Hartford Town Hall, Department of Planning and Development Services, 171 Bridge Street, White River Junction, Vermont (802) 295-3075. Persons wishing to appear and be heard may do so in person, or be represented by another party and/or submit written testimony.

An interested person who has **participated** in a municipal regulatory proceeding may appeal a decision rendered in that proceeding to the Vermont Environmental Court. Participation in a local regulatory proceeding shall consist of offering, through oral or written testimony, evidence or a statement of concern related to the subject of the proceeding.

Steve Lagasse, Chair

**Zoning Board of Adjustment
Administrative Agenda
July 24, 2024**

1. Public Comment

2. Minutes

3. Administrative Permits

None

4. Discuss starting Hearings later

5. Availability for the next Public Hearing (September 4th)

**DRAFT
MINUTES
Zoning Board of Adjustment
June 26, 2024**

Present: Steve Lagasse, Chair, Chris Lowe, Vice Chair, Alice Maleski, Tom Franklin, Lannie Collins, Selectboard Liaison, and Jo-Ann Ells, Zoning Administrator

Absent: Stephanie Gile

Public Hearing

Chris Lowe opened the Hearing, read the hybrid meeting script, and took a roll call.

Administrative Matters

1. Public Comment

None.

2. Minutes

Steve Lagasse moved to approved the minutes of May 15, 2024. Tom Franklin seconded, and the motion passed unanimously.

3. Administrative Permits

Steve Lagasse reviewed an administrative permit issued to the U.V. Haven for a 55 sq. ft addition to food shelf and a shed.

4. Availability for the next Public Hearing (July 24, 2024)

Chris Lowe noted that he might not be available for the Hearing.

Steve Lagasse reviewed the Hearing process, interested party rule, and issued the oath.

Alice Maleski arrived.

Public Hearing

1. Application #05-24 by the State of Vermont (owner/applicant) for Conditional Use Approval under section 260-16 of the Hartford Zoning Regulations for approval of additions and improvements to the Windsor District Court, lot 46-0024-001, 82 Railroad Row, White River Junction, in CB and Design Reivew zoning districts.

Joe Aja and Sherilyn Lafley were present.

06-26-24

Jo-Ann Ells reviewed several change to the draft Findings of Fact.

Sherilyn Lafley gave an overview of the project.

Tom Franklin asked about the color of the proposed terracotta tiles. Sherilyn Lefley noted they would be similar to the existing brick.

It was noted that EV chargers were proposed in the public parking area and conduit for EV charger would be installed in the employee parking area.

There was no public comment.

Steve Lagasse moved to close the Public Hearing. Alice Maleski seconded, and the Public Hearing was closed.

Alice Maleski moved to approve the application with the noted changes. Chris Lowe seconded, and the motion passed unanimously.

2. Application #02-24 by Guernsey Ruth Hall Revocable Trust (owner) and Summit Distributing (applicant) for Conditional Use Approval under section 260-16 of the Hartford Zoning Regulations for approval of a car wash, lot 44-0041-000, Sykes Mountain Avenue, White River Junction, in an IC zoning district.

Tom Frawley, Nicole Duquette, Aaron Farbo, and Bob Bollinger were present.

Jo-Ann Ells noted that a third party review of the noise study had been completed. She reviewed several changes to the draft Findings of Fact.

Nicole Duquette reviewed changes to the application since the Board's initial review.

It was noted that silencers on the car wash blowers and a sound barrier wall would be added to mitigate noise.

The proposed landscaping was reviewed.

Chris Lowe shared that he did a site visit to the existing car wash on Sykes Mountain Avenue when a vacuum, the self-wash bay, and automatic wash were all in use and observed that the activity did not generate a lot of noise. He added that the thought the plan looked good.

Tom Franklin stated that he was happy to see that the neighbors' concerns regarding runoff were addressed.

Alice Maleski commented that she was impressed with the changes to the application. Steve Lagasse echoed the comment and thanked the applicant for responding to the concerns voiced by neighbors.

06-26-24

There was a discussion regarding vehicular circulation. Nicole Duquette offered that employees will be on site to assist with traffic flow if needed.

Steve Lagasse asked for public comment.

Lannie Collins asked if water would be reused. Nicole Duquette stated that a percentage of the water will be reused. It was noted that the DPW has reviewed and commented on the application and any discharge into the Town's system will be as approved by the DPW.

Steve Lagasse moved to close the Public Hearing. Alice Maleski seconded, and the Public Hearing was closed.

Alice Maleski moved to approve the application with the noted changes. Chris Lowe seconded, and the motion passed unanimously.

Adjournment

The Board adjourned at 7:20 p.m.

Respectively Submitted,

Alice Maleski, Clerk

Zoning Board of Adjustment Members/Terms:

Stephanie Gile	11-15-24
Alice Maleski	03-07-25
Steve Lagasse	03-07-25
Chris Lowe	03-21-25
Tom Franklin	04-04-25

DRAFT FINDINGS OF FACT CONCLUSIONS OF LAW AND DECISION

HARTFORD ZONING BOARD OF ADJUSTMENT

Application #07-24 by 12 Tremont Street, LLC and Tim Sidore for Conditional Use Approval to convert an existing structure into a multi-unit dwelling lot 09-0072-000, 1200 Christian Street Hartford, in a proposed Planned Development

This decision pertains to application #07-24 by 12 Tremont Street, LLC (owner) and Tim Sidore (applicant) for Conditional Use Approval under section 260-16 and 260-25 of the Hartford Zoning Regulations to convert an existing structure into a multi-unit dwelling, lot 09-0072-000, 1200 Christian Street, Hartford, in a proposed Planned Development.

FINDINGS OF FACT

Based upon the information contained in the exhibits for this application (see attached list) and the evidence adduced at the Zoning Board of Adjustment meeting of July 24, 2024 the Zoning Board of Adjustment makes the following Findings of Fact:

General Information

1. The owner of record of lot 09-0072-000 is 12 Tremont Street, LLC, who acquired the lot on 08-17-23. The deed is recorded in volume 614, page 623 of the Hartford Land Records.
2. The lot is developed with the former “Brookside Nursing Home” including a garage.
3. The lot is approximately 3.95 acres.
4. The lot is located at 1200 Christian Street.
5. The lot abuts Christian Street on one side and Brookside Drive on two sides.
6. The lot is class one (1), municipal water and wastewater.

This Application

7. The applicant proposes to convert the existing building to 38 dwelling units comprised of 22 studio units and 16 one-bedroom units. The existing garage is proposed to be used for maintenance associated with the property.

According to the applicant, “the building itself will feature generous 8’ wide, skylit hallways; near the main south entry a small community lounge space looks out onto the courtyard. The apartments, though small, will be well appointed with quality materials and fixtures. The walls, with their concrete block structure, minimize sound transmission and will keep the apartments quiet. Laundry facilities will be provided in some apartments and in publicly accessed laundry rooms. The high-ceiling basement will provide additional tenant storage and depending on demand, could be repurposed to provide exercise and game room space.” (See exhibit #6)

8. The lot is in an R-3 zoning district. Multi-unit dwellings are not allowed in the underlying R-3 zoning district.

The Zoning Regulations allow the Planning Commission to approve multi-unit dwelling as a Planned Development.

In May of 2024, the applicant obtained preliminary approval of a Planned Development. The applicant has applied for final approval of the Planned Development. The application is pending Planning Commission review.

Multi-unit dwellings are a Conditional Use in the applicant’s proposed Master Plan for the Planned Development.

This application (#07-24) is conditioned on the approval of the Planned Development by the Planning Commission.

9. The *Planning Commission* has the authority to grant a density bonus under 260-47 (C) of the Zoning for Planned Developments so long as the total number of such dwelling units does not exceed one hundred twenty-five (125) percent of the permitted number of units.

22 dwelling units are possible based on the density calculation in the R-3 zoning district $(172,062/8,000 = 21.5)$ ¹

28 dwelling units are possible with a density bonus under 260-47 C. $(22 \times 1.25 = 27.5)$

¹ Hartford Zoning Regulations, § 260-18, Dwelling unit density. In every case, the Administrative Officer shall determine the maximum number of dwelling units permitted on any lot by dividing the lot size, provided by a survey (furnished by the owner and sealed by a Vermont registered land surveyor), by the minimum area per dwelling unit. standards for the district in which the lot is located. In all cases such determination shall be rounded to the nearest whole number.

In 2023, the Vermont Legislature enacted Act 47 “An act relating to housing opportunities made for everyone.”

24 V.S.A. 4412 (13) states:

In any area served by municipal sewer and water infrastructure that allows residential development, bylaws shall permit any Affordable Housing Development, as defined in subdivision 4303(2) of this title, including mixed-use development, to exceed density limitations for residential developments by an additional 40 percent, which shall include exceeding maximum height limitations by one floor, provided that the structure complies with the Vermont Fire and Building Safety Code.

24 V.S.A. § 4303 (2) defines Affordable Housing Development as a housing development of which at least 20 percent of the units or a minimum of five units, whichever is greater, are affordable housing units. Affordable units shall be subject to covenants or restrictions that preserve their affordability for a minimum of 15 years or longer as provided in municipal bylaws.

24 V.S.A. 4303 (B) defines Affordable Housing Units as:

Rental housing for which the total annual cost of renting, including rent, utilities, and condominium association fees, does not exceed 30 percent of the gross annual income of a household at 80 percent of the highest of the following:

- (i) the county median income, as defined by the U.S. Department of Housing and Urban Development;
- (ii) the standard metropolitan statistical area median income if the municipality is located in such an area, as defined by the U.S. Department of Housing and Urban Development; or
- (iii) the statewide median income, as defined by the U.S. Department of Housing and Urban Development.

The applicant seeks to develop Affordable Housing Development, meeting the requirements outlined above. Therefore, 39 units are possible ($28 \times 0.40 = 11.2$)
($28 + 11 = 39$)

According to the applicant, they “pledge to designate 40% of the units as ‘affordable/workforce housing,’ based on 24 V.S.A. 4412 (13) , 24 V.S.A. § 4303 (2), and 24 V.S.A. 4303 (B).” Given this, 15 of the units will be “Affordable” as defined above. ($38 \times 0.40 = 15.2$)

10. As outlined in the Master Plan for the Planned Development, “Greater than 50% of the development will remain in open space, as defined by Hartford Zoning Regulations for public and or common usage.”

11. Project Access

Christian Street

The applicant proposes to continue use of the curb cuts on Christian Street for access.

The applicant proposes to restrict use of the southern curb cut on Christian Street to an entrance only and restrict use of the northern curb cut on Christian Street to an exit only.

Brookside Drive

The applicant proposes to restrict use of the existing curb cut on the west side of the lot from Brookside Drive to maintenance and emergency vehicles. A gate will be installed to prevent access from this curb cut to the remainder of the lot.

The applicant will coordinate access to the gate with the Hartford Fire Department.

The applicant understands that the curb cuts/driveway entrances need to be improved to meet the Hartford Transportation Ordinance.

(See exhibit #2)

12. According to the applicant, they have reached out to Advance Transit regarding a bus stop near the project. To date, they have not received a response.
13. Section 260-25 B of the Zoning Regulations, Protection of Surface waters states:

A riparian buffer shall be maintained for a minimum of 100 feet from the top of the bank of the Connecticut, Ottauquechee and White Rivers, or for a minimum of 30 feet from the edge of a stream or any other surface water identified on the most recent edition of the Hartford G.I.S. Natural Resources Map. The riparian buffer also includes the area between the river and the top of the bank. No development, excavation, filling, clearing or grading shall occur within the buffer without conditional use approval from the Zoning Board of Adjustment except as specified in Subsections D and E.

There is a stream on the lot is identified on the most recent edition of the Hartford G.I.S. Natural Resources Map.

The riparian buffer will be managed in accordance with sections 260-25 D and 260-25 E of the Zoning Regulations.

The applicant proposes to install a dumpster pad and stormwater management in the

riparian buffer.

According to the applicant, “the dumpster pad encroaches by approximately 280 sf +/-, all other disturbed areas within the riparian buffer are part of existing parking areas to be maintained as parking and minor disturbances for stormwater management, a VT ANR wetland permit will be required for this and other work in the wetland buffer.

The dumpster pad will be made of concrete and is sloped so that the run-off runs away from the wetlands to a stabilized outfall.

The location of the dumpster is the best possible location given the site constraints and proximity to existing parking areas. Other locations on the site would constrain emergency vehicle access and reduce parking. Disturbed areas will be replanted with dense native vegetation, steep slopes will be stabilized with riprap and or biodegradable erosion control netting and restored with native vegetation. The erosion prevention and sediment control plan will be followed during construction in accordance with the VTDEC EPSC Handbook.”

14. There are wetlands on the property. The wetlands and a 50’ wetland buffer as required the State are shown on the plans.

According to the applicant, they “are in discussion with the Department of Environmental Services and will be applying for a wetlands permit if the project is approved. We agree in that DES will likely want minimal management of the forest here because of the nature of wetlands on this property. We have included riparian native VT species to add around the perimeter of the developed portion of the landscape as a measure to go back toward a natural environment within the wetland buffer areas. An additional column on the LA plan set was added to the plant list to indicate the riparian varieties.”

15. According to the applicant, site amenities include “the natural beauty of the site with its brook, a pond, generous lawn and thick woods, as well as a small courtyard with a gazebo and outdoor cooking facilities. Existing trees and shrubs will be selectively pruned; a wide variety of new trees and shrubs and 3,400 SF of wildflowers will be planted. We would like to add a small, fenced-in area at the south end of the property, near to the culvert, creating a dog friendly park. The dog park will have a pea stone surface and a 6” gravel base to filter and contain dog waste.” (See exhibit #2, landscape detail on L3.2)

16. The applicant has not submitted a professional traffic study.

According to the applicant, “Under our proposal, we are reducing density from the previous 68 bed nursing home. Fewer people living onsite, and without all of the employees, caregivers and vendors, the parking and use of municipal resources

will be significantly less. We usually have .8 cars/studio unit parked each night at our other all studio buildings, including our 69- unit apartment building at 132 S. Main Street in WRJ and our 43-unit building at 75 Bank Street—the former Lebanon Junior High.

Assuming a full occupancy at all times with 38 persons and no staff, the 8th Edition of the ITE Trip Generation Report calculates the project would generate **126 daily trips**.

According to that same ITE report, the prior 68-bed nursing home—with 12 staff on site at any one time-- would generate **209 daily trips**. The total of 12 staff is based on Federal rules issued 4/2024 that requires 3.48 hours of nursing/aide care per patient. This translates to 10 health staff *on average* on site at any one time for full occupancy; assuming an additional 2 maintenance staff, the total comes to 12 coming and going on 3 shifts per day, 7 days per week. (Note, according to the 2001 Federal Report on Staffing Standards, <https://nursinghome411.org/federal-staffing-standard/>, staffing for nurses and aides should total 4.1 hours per resident per day *to meet the bare minimum* for resident safety.)”

17. The Zoning Regulations require two parking spaces per unit, which results in a requirement of 76 parking spaces. (38 x 2 = 76)

The applicant is requesting that the *Planning Commission* approve a permanent reduction in the required parking spaces for the project from 76 to 46 including 3 accessible parking spaces.

Electrical conduit for future electric vehicle charging stations will be installed for forty parking spaces.

Section 260-24(9)(e) allows for modifications in the number of required parking spaces as follows to be approved by the *Planning Commission* as follows:

If, in site plan approval proceedings or at any time in the future after a duly warned hearing, the Planning Commission determines that unique usage or special conditions exist which warrant increasing or reducing the need for parking, it may require the provision of off-street parking spaces up to 50% more or less than the requirements in Subsection C(9)(a) above. In the Central Business District, the parking may be reduced beyond 50%. When approving an increase or reduction in any zoning district, the Planning Commission shall consider the following:

- [a] Is the site located on or within 1,000 feet of a transit route?
- [b] Are there shared parking facilities with abutting businesses which are sufficient to meet parking demand?
- [c] For mixed-use projects, do the proposed uses have staggered business hours with minimal overlap?

- [d] Does the type of business proposed generate substantial pedestrian traffic and are adequate pedestrian facilities present?
- [e] Does an employer-sponsored "transit pass" program, or vanpool, carpool or rideshare program exist, or can one be established (as a condition of the approval) to substantially reduce the need for on-site parking?
- [f] Is safe and adequate on-street parking available?
- [g] Is parking in municipal parking lots available?
- [h] Does the Institute of Traffic Engineers, American Planning Association or other professional source show parking demand requirements for the proposed use(s) that are different than the standards specified in these regulations?
- [i] Is there other information that justifies a change in the parking requirements?

According to the applicant, the reduction request is based on the following:

- The applicant will provide an electric vehicle for shared use by tenants.
- This project will include more than one parking space per unit. Lease agreements will limit the number of parking spaces to one space per unit, and one vehicle per unit will be allowed to park onsite.
- Each dwelling units will have an assigned parking space.
- 24 V.S.A. ss. 4414 (4) (effective December 2024), states

Parking and loading facilities. A municipality may adopt provisions setting forth standards for permitted and required facilities for off-street parking and loading, which may vary by district and by uses within each district. In any district that is served by municipal sewer and water infrastructure that allows residential uses, a municipality shall not require more than one parking space per dwelling unit. However, a municipality may require 1.5 parking spaces for duplexes and multiunit dwellings in areas not served by sewer and water and in areas that are located more than one-quarter mile away from public parking rounded up to the nearest whole number when calculating the total number of spaces. These bylaws may also include provisions covering the location, size, design, access, landscaping, and screening of those facilities. In determining the number of parking spaces for nonresidential uses and size of parking spaces required under these regulations, the appropriate municipal panel may take into account the existence or availability of employer "transit pass" and rideshare programs, public transit routes, and public parking spaces in the vicinity of the development.

18. According to the applicant, “We are providing 3 accessible parking spaces (2 by the accessible entrance on the north side and 1 for the units at the basement level) directly adjacent to the entries they serve. A nearly continuous walk is provided around the north, west and east sides of the building, continuing into the courtyard/gazebo/BBQ/deck area. In addition, a 5’ wide accessible pathway leads to the southernmost parking. A stonedust path leads to and around the dog park, the open and wooded areas to its south and the southeasternmost tip of the parking lot. As shown on the photometric plan, all seven building entries are well lit, including the primary entry on the south, and the courtyard on the east. Adequate but neighborhood sensitive lighting has been provided for the parking and drive aisles. There is parking for 6 bikes located adjacent to the north entry.”
19. The applicant proposes to delineate a loading/unloading area for delivery vehicles adjacent to the primary entrance to the building.
20. The applicant proposes to install bike racks on the north side of the building to accommodate six bikes.
21. The applicant provided a landscape plan.

According to the applicant, “the project plans include a 2-year guarantee on all proposed landscape plantings. In addition, all varieties specified are VT native and hardy varieties that will thrive on their own once established. The owner has an in-house maintenance team that will provide regular landscape maintenance including mowing lawn areas, pruning, mulching beds annually, etc.”

(See exhibit #2)

22. The applicant has provided a photometric plan.

According to the applicant, “all fixtures will be dark- sky compatible.”

The applicant understands that lighting must not create excessive off-site or sky glow. If the Zoning Administrative Officer determines that lighting does not meet this condition, the applicant will be required to comply.

(See exhibit #2)

23. The applicant submitted turning movement plans. (See exhibit #2)
24. The applicant proposes to install a gate to restrict access from Brookside Drive to maintenance personnel and emergency vehicles.
25. According to the applicant, “a portion of stormwater runoff will be infiltrated into the ground and another portion will be treated in the grass swale with check dams along the south edge of the parking area. Along Brookside Drive in front of the building overall

impervious surface has been reduced.

Overall site impervious is expected to be increased by approximately 2,600 sf. Stormwater attenuation will be provided with underground stormwater detention chambers and infiltration. A curb will be provided along the south edge of pavement at the upper driveway off Christian St. to collect and treat runoff in the underground detention system. In addition, a portion of rooftop surfaces will also be collected in the same underground chamber system. Post development stormwater runoff rates are expected to be reduced upon completion of the stormwater management plan.”

26. Section 260-27 C of the Hartford Zoning Regulations states:

C. Grading and drainage. The following provisions shall apply to any changes which affect drainage of water to or from a property:

- (1) Any changes in grading shall be made so that runoff is directed to established drainage courses and will not cause ponding or flooding of other properties, or exceed the capacity of downstream drainage facilities.
- (2) When possible, drainage systems shall be designed so that water runoff existing prior to site development shall not be increased after development. When possible, drainage systems shall be designed so that water runoff existing prior to site development shall not be increased after development.
- (3) All changes in grade shall be controlled so as not to cause a nuisance or damage to other properties or erosion of topsoil.

According to the applicant, “Pursuant to section 260-27, part C of the Town of Hartford Zoning Regulations, the site grading has been designed so that runoff is directed to established drainage swales and curbing and will not cause ponding or flooding of other properties or exceed the capacity of downstream drainage facilities. The drainage system will be designed so that water runoff existing prior to site development will not be increased after development. All changes in grade will be controlled so as not to cause a nuisance or damage to other properties or erosion of topsoil.”

27. The applicant proposes to connect to municipal water and sewer in accordance with Hartford’s Water and Wastewater Rules and Regulations.
28. Snow will be stored on site and will be removed as needed. Snow will not be stored in such a manner as to create a hazard.
29. The applicant proposes to install a fenced dumpster area.

30. The applicant will schedule a preconstruction meeting with the DPW to review project schedule, design, and construction issues at least two weeks before actual site work commences.
31. The applicant understands that a permit from the DPW must be obtained before any work is done within the Town ROW.
32. During construction of the proposed project erosion and sediment control measures as specified in the most current edition of the *Low-Risk Site Handbook for Erosion Prevention and Sediment Control* published by the Vermont Department of Environmental Conservation will be followed.
33. According to the applicant, the building will meet applicable Vermont Energy Code Standards. An Energy Code Certificate will be recorded in Hartford's Land Records before a Certificate of Occupancy will be issued.
34. Stockpiling of materials related to the construction of the project will be located twice the distance of the height of the stockpiled materials from any public or private right-of-way or property boundary.
35. Construction trailers may be used on the condition that they are placed a minimum of ten feet from any property line, are placed only when there is an effective zoning permit for the project, and are removed before a Certificate of Occupancy is requested. No construction trailer may be placed in such a way that it obstructs sight distance along the traveled way, at any intersection or driveway. Any construction trailer found to obstruct sight distance will be required to be relocated immediately.
36. The applicant understands that no structure, signage, parking spaces or plantings may be placed in such a way that obstructs sight distance along the traveled way, at any intersection or driveway. Any signs, parking spaces or plantings found to obstruct sight distance will be required to be relocated.
37. Construction of the project will comply with Section 260-31 B of the Hartford Zoning Regulations, which states:

Earth moving and hauling, rock drilling or crushing, jack hammer and similar excessively loud equipment shall not be operated on Sundays or between the hours of 5:30 p.m. and 7:00 a.m. except in emergency situations.
38. The applicant understands that they retain the obligation to identify, apply for, and obtain any relevant local and state permits for this project.
39. The applicant understands that all activities authorized by the issuance of the zoning permit shall be started within six (6) months and completed within two (2) years of its date of issue, or the zoning permit shall become null and void and reapplication to complete any activities shall be required. Any activities not completed within the above

permit period shall be subject to any ordinance in effect at the time of reapplication. One six (6) month extension to both time periods may be granted by the Planning Commission.

40. The applicant understands that they must obtain a Certificate of Occupancy from the Zoning Administrative Officer upon completion of the project and before the site may be occupied. The applicant understands that a Certificate of Occupancy will not be issued if the project is not in compliance with these Findings, Plans and any conditions placed on the decision.

CONCLUSIONS OF LAW

The Hartford Zoning Board of Adjustment, based upon the forgoing Findings of Fact, concludes that the proposed project will not adversely affect:

260-16A (1) Community facilities.

(a) The capacity of planned or existing **community facilities**, including, but not limited to:

- [1] Emergency services.
- [2] Municipal water or sewer system.
- [3] Recreational facilities.
- [4] The school system.
- [5] Solid waste disposal facilities.

260-16A (2) The **character** of the area, as defined by the objectives of the zoning district within which the project is located and specifically stated policies and standards of the municipal plan, including, but not limited to:

- (a) Scenic or natural beauty, historic sites, or rare and irreplaceable natural areas.
- (b) Compatibility with scale and design of structures existing in that area.

260-16A (3) The capacity of **roads and highways** in the vicinity to safely accommodate expected traffic flows. In making this determination, the Board may require submission of a traffic impact study made by a professional traffic engineer.

260-16A (4) Any Town of Hartford **bylaws** in effect.

260-16A (5) Utilization of **renewable energy resources**.

DECISION

The Hartford Zoning Board of Adjustment, based upon the forgoing Findings of Fact and Conclusions of Law, hereby approves application #07-24 by 12 Tremont Street, LLC (owner) and Tim Sidore (applicant) for Conditional Use Approval under section 260-16 and 260-25 of the Hartford Zoning Regulations to convert an existing structure into a multi-unit dwelling, lot 09-0072-000, 1200 Christian Street, Hartford, in a proposed Planned Development **on the**

condition that:

1. Before a zoning permit is issued:

- **The applicant obtains final approval of the Planned Development from the Planning Commission and obtains a Zoning Permit to finalize the approval.**
- **The applicant submits covenants/restrictions outlining compliance with 24 V.S.A. § 4303 (2) for review and approval by the Chair. The information must include a process for the applicant to demonstrate compliance on an annual basis, and include any guidance on compliance provided by the State. Once approved, the applicant must record the document in Hartford's Land Records and provide a copy to the Zoning Administrator.**
- **The plans are amended to include a defined access route from the parallel parking spaces adjacent to Brookside Drive to the building's entrance for review and approval by the Chair.**
- **The landscape plan is amended to correctly identify the location of the "mulch bed area" located on the western side of the building along Brookside Drive.**
- **The applicant submits a copy of their water and wastewater Allocation from the DPW.**
- **The plans are amended to include the charging station for the shared electric vehicle.**
- **The applicant provides information from Advance Transit regarding their ability to add a bus stop near the project.**
- **A final set of plans is submitted to the Zoning Administrator accompanied by verification from the applicant's Engineer, Landscape Architect, and Architect confirming that changes to the plans approved were limited to those required by the Zoning Board of Adjustment.**

2. Before a Certificate of Occupancy is issued, the applicant submits to the Zoning Administrator:

- **Written verification from the Fire Department that the applicant has coordinated access to the gate with the Department.**
- **Written verification from a Professional Engineer licensed by the State of Vermont certifying that the project was constructed in compliance with the approved civil plans and Findings of Fact.**

- **Written verification from their Landscape Architect certifying that the plantings and amenities were installed in compliance with the approved plans and Findings of Fact.**
- **Written verification from their Architect certifying that the building was constructed in compliance with the approved architectural plans and Findings of Fact.**
- **Written documentation that the water and sewer have been approved by the DPW.**
- **Written verification from the DPW confirming that they have received acceptable record drawings.**
- **An electronic copy of the record drawings.**
- **A copy of the Energy Code Certificate as recorded in Hartford's Land Records.**

This decision is also subject to the condition that no modification or expansion shall be made to the project as described and limited in the above Findings of Fact and Conclusions of Law without the written approval of the Hartford Zoning Board of Adjustment and/or the Hartford Planning Commission. This decision may be revoked, and a new application or permit required if conditions and findings are violated or modified without written approval.

The applicant is responsible for insuring compliance with all applicable State and Federal regulations pertaining to this application.

Those in favor:

Those in opposition:

Those not participating:

Dated at Hartford, Vermont this ____ day of July 2024.

HARTFORD ZONING BOARD OF ADJUSTMENT

By: _____
Chairperson/Vice-Chairperson

Appeal Rights: An interested person who has participated in the proceeding related to this decision has the right to appeal this decision to the Vermont Environmental Court, pursuant to 24 V.S.A. s. 4471 and V.R.E.C.P.5, in writing, within 30 days of the date of this decision. If you fail to appeal this decision, your right to challenge this decision at some future time may be lost

because you waited too long. You will be bound by this decision, pursuant to 24 V.S.A. s. 4472 (d) (exclusivity of remedy; finality).

EXHIBIT LIST

1. Application #13-23
2. Set of Plans including
 - Survey prepared by DiBernardo Associated, LLC
 - Civil Plans prepared by Engineering Ventures including:
 - C1.0 Demolition Plan
 - C1.1 Erosion Prevention and Sediment Control plan
 - C1.2 Garbage Truck Turning Plan
 - C1.3 Fire Truck Turning Plan
 - C1.4 Fire Truck Turning Plan (2)
 - C2.0 Site Plan Overall
 - C2.1 Site grading and Drainage Plan
 - C3.0 Site Details
 - C3.1 Drainage Details
 - C3.2 Erosion Prevention and Sediment Control Details (1)
 - C3.3 Erosion Prevention and Sediment Control Details (2)
 - PH1 Site Photometrics Plan
 - Landscape Plans prepared by Park Architecture including:
 - L1.0 Existing Conditions & Site Preparation Plan
 - L1.1 Existing Conditions
 - L2.0 Site Landscape Plan
 - L2.0 Site Landscape Plan (color version)
 - L3.0 Landscape Details
 - L3.1 Landscape Details
 - L3.2 Landscape Details
 - Architectural Plans prepared by Studio Nexus including:
 - SD1 Existing Floor Plan
 - SD2 Proposed Floor Plans
 - SD3 Renderings
3. Letter from Jesse Klymn, Anika Gilbert, Frank Klymn, Jessica and Jay McDonough, Carey O'Neil, John O'Neil, Emily and Dillon Owens, Benjamin Davis, Susan Goodwin, Deborah Rodgers, Jeff Arnold, Hilde Ojibway, Michael and Dorothea Barton, Jenny Horn, Jeni and Jeff Frechette, Mark and Kathy Detzer, Marie Wesson, Ann Kerigan,

Susan Loskutoff, and John and Kathy Wetzel dated July 8, 2024

4. Letter from Michael and LeeAnne Heeremans dated July 8, 2024
5. Memo from Jo-Ann Ells dated July 2, 2024 with applicant's response of July 8, 2024
6. Project Narrative dated June 24, 2024, updated July 7, 2024



Town of Hartford

Planning Commission App # _____
Zoning Board of Adjustment App # _____

APPLICATION TO PLANNING COMMISSION &/OR ZONING BOARD OF ADJUSTMENT

PLEASE COMPLETE ALL ITEMS APPLICABLE AND PROVIDE THE CORRECT NUMBER OF COPIES OF THIS APPLICATION AND THE SITE PLAN OR SUBDIVISION PLAT. ABUTTING LANDOWNERS WILL BE NOTIFIED BY THE PLANNING/ZONING OFFICE.

1. **NAME & ADDRESS OF RECORD OWNER(S)** 12 Tremont Street, LLC
77 Bank Street, Lebanon, NH 03766
Deed Recorded Book 9 Page 72 (Available at Town Clerk's Office)
Telephone: Work (603) 322-3260 Cell same Email Address: tim@ledgeworks.com
2. **NAME & ADDRESS OF APPLICANTS(S)** Tim Sidore
77 Bank Street, Lebanon, NH 03766
Telephone: Work _____ Cell _____ Email Address: mike@ledgeworks.com
3. **NAME & ADDRESS OF APPLICANTS' CONSULTANT** Jim Wasser, Studio Nexus Architects & Planners
PO Box 275, WRJ, VT 05001
Telephone: Work (802) 275-5110 Cell _____ Email Address: jimw@studionexusarch.com
4. **PROJECT LOCATION** 1200 Christian Street
5. **PRESENT USE OF PROPERTY** nursing home
6. **BRIEFLY DESCRIBE PROJECT** (in addition, a detailed narrative must be submitted)
Convert nursing home to planned unit development with 38 residential apartments. The
existing building footprint will remain as is, but with interior and exterior improvements.
7. **PROJECT SIZE IN SQ. FT OR ACREAGE** 3.95 acres
Lot Width _____ Lot Depth _____
8. **IS PROJECT ON TOWN WATER/SEWER?** Yes X No _____ If Yes, estimated
Water Consumption/day _____ Amount of Sewer Flow Projected _____
Size of Closest Lines Above (water) _____ (sewer) _____
9. **DESCRIBE LANDSCAPING PLAN** See attached. New landscaping will be added throughout
and much of the existing landscape will be preserved.
10. **IF A BUSINESS** - Present # of Employees _____
Proposed Days & Hours of Operation _____
Will you use the landfill & recycling center? Yes _____ No _____
The main waste by-product of your business is: _____

11. **IF A RESTAURANT** - Present seating _____ Proposed _____
12. **PARKING SPACES** - Number to be provided 46
13. **SIGNAGE** - Free Standing _____ Attached _____
14. **FENCING** – Location _____ Height _____
15. **NEAREST WATER SUPPLY FOR FIREPROTECTION** on site (sprinkler service)
16. **ESTIMATED VALUE OF PROPOSED WORK** \$1,800,000
17. **IS THERE AN EXISTING DRIVEWAY?** yes
18. **IF A SUBDIVISION** - Number of New Lots to be Created _____
- Do you request that the Town consider taking over maintenance of – Roads no
- Or Park/Open Space _____

I HEREBY CERTIFY THAT THE INFORMATION PROVIDED ON THIS APPLICATION IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE. (This certification must be signed by lot owners and applicants).

Jimmie B. Bidore 21 June 2024
Applicant's Signature Date

Jimmie B. Bidore 21 June 2024
Lot Owner's Signature Date

Co-Applicant's Signature Date

Co-Owner's Signature Date

The applicant retains the obligation to identify, apply for, and obtain any relevant local or state permits before any construction may commence. Please call 802-282-6488 to speak with the State Permit Specialist.

For Office Use Only		
Map/Lot #		
Fee Paid \$:		Receipt #:
PC: Site Plan	Min. Sub.	Maj. Sub.
PD	PD Amend.	Other
ZBA: Conditional Use	Variance	Appeal
Other		
Zoning District (s)		Date Filed:
Official Submittal Date:		

EXECUSUITE LLC

Bringing new life to our historic downtowns

Brookside Apartments, 1200 Christian Street, Narrative

June 24, 2024; Updated: July 7, 2024

Formerly known as the Brookside Nursing Home, the existing facility is located on the busy Christian Street corridor with access to major routes including route 5, route 4, I-89 and I-91. The 19,500 sq. ft. facility sits on 3.95+/- acres and includes the original 2-story facility built in 1925, the primary one-story building constructed in the early 70's and a remote maintenance garage constructed in 1980. Inside features include the original nursing home rooms (some with baths), a large activity center room, a separate wing of office spaces and a commercial style kitchen in the basement. Outside there are paved parking spaces on both sides of the building, a delivery entrance and a 1500+/- sq. ft. garage/outbuilding.

Our preference is to convert the existing facility to a multi-unit dwelling. The site is zoned R-3, which requires a minimum of 8,000 SF per unit. With a PUD and a density bonus of 25%, Brookside's 3.95 acre lot would allow $3.95 \text{ acres} \times 43,560 \text{ SF/acre} / 8,000 \text{ SF/unit} = 22 \text{ units} \times 1.25 = 28 \text{ units}$. With a 40% affordable housing bonus, the total allowable is 39 units. The density bonus as described in 260.47 (B-C) is appropriate given this project's merits and the housing needs for the Hartford community. Specifically, 76% of all households in Hartford consist of 1-2 persons as detailed in a 2020 survey conducted by American Community Survey (Tables S2504 and S2501) and referenced in Hartford's 2023 draft housing chapter of the Town Plan. That same study notes that only 18.7% of Hartford's current housing stock is 0-1 bedroom. The Brookside redevelopment utilizes the existing facility while preserving the natural beauty of its surroundings, both on the property and in the neighborhood. The efficient use of resources, both natural and constructed, promotes the harmonious use of property while preserving the existing topography, tree cover and geology.

Working entirely within the existing building footprint, we envision a redevelopment that creates 38 smaller studio and one-bedroom apartments. We anticipate our residents will be 50% area professionals and graduate students and 50% service industry, support/administrative and trades workers. 40% of units will be offered at below market, affordable rents, in line with Twin Pines Housing rent levels. Easy cycling opportunities and a shared electric vehicle powered by on site solar generation will reduce parking/trips/carbon footprint.

Site amenities at the redeveloped facility include the natural beauty of the site with its brook, a pond, generous lawn and thick woods, as well as a small courtyard with a gazebo and outdoor cooking facilities. Existing trees and shrubs will be selectively pruned; a wide variety of new trees and shrubs and 3,400 SF of wildflowers will be planted. We would like to add a small, fenced-in area at the south end of the property, near to the culvert, creating a dog friendly park.

We are providing 3 accessible parking spaces (2 by the accessible entrance on the north side and 1 for the units at the basement level) directly adjacent to the entries they serve. A nearly continuous walk is provided around the north, west and east sides of the building, continuing into the courtyard/gazebo/BBQ/deck area. In addition, a 5' wide accessible pathway leads to the southernmost parking. A stonedust path leads to and around the dog park, the open and wooded areas to its south and the southeasternmost tip of the parking lot. As shown on the photometric plan, all seven building entries are well lit, including the primary entry on the south, and the courtyard on the east. Adequate but neighborhood sensitive lighting has been provided for the parking and drive aisles. There is parking for 6 bikes located adjacent to the north entry.

Under our proposal, we are reducing density from the previous 68 bed nursing home. Fewer people living onsite, and without all of the employees, caregivers and vendors, the parking and use of municipal resources will be significantly less. We are providing 46 parking spaces, a reduction of approximately 7 spaces from what was there previously. 40 of those spots will be EV Capable Spaces. We added 4 parking spots along Brookside Drive from what was approved for the PUD. They are necessary to provide some additional accommodation for folks with limited mobility (the southern entry is not accessible) and because the bulk of the housing units (21) are located in the northern portion of the building.

We usually have .8 cars/studio unit parked each night at our other all studio buildings, including our 69-unit apartment building at 132 S. Main Street in WRJ and our 43-unit building at 75 Bank Street—the former Lebanon Junior High. However, assuming a full occupancy at all times with 38 persons and no staff, the 8th Edition of the ITE Trip Generation Report calculates the project would generate **126 daily trips**. According to that same ITE report, the prior 68-bed nursing home—with 12 staff on site at any one time--would generate **209 daily trips**. The total of 12 staff is based on Federal rules issued 4/2024 that requires 3.48 hours of nursing/aide care per patient. This translates to 10 health staff *on average* on site at any one time for full occupancy; assuming an additional 2 maintenance staff, the total comes to 12 coming and going on 3 shifts per day, 7 days per week. (Note, according to the 2001 Federal Report on Staffing Standards, <https://nursinghome411.org/federal-staffing-standard/>, staffing for nurses and aides should total 4.1 hours per resident per day *to meet the bare minimum* for resident safety.)

The building itself will feature generous 8' wide, skylit hallways; near the main south entry a small community lounge space looks out onto the courtyard. The apartments, though small, will be well appointed with quality materials and fixtures. The walls, with their concrete block structure, minimize sound transmission and will keep the apartments quiet. Laundry facilities will be provided in some apartments and in publicly accessed laundry rooms. The high-ceiling basement will provide additional tenant storage and depending on demand, could be repurposed to provide exercise and game room space.

This project can be completed quickly, providing critically needed housing units to the area inventory; in turn existing multi-bedroom housing stock will open up for families and those seeking larger housing options.

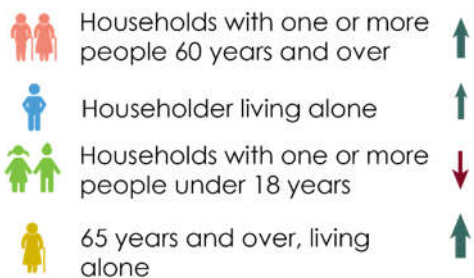
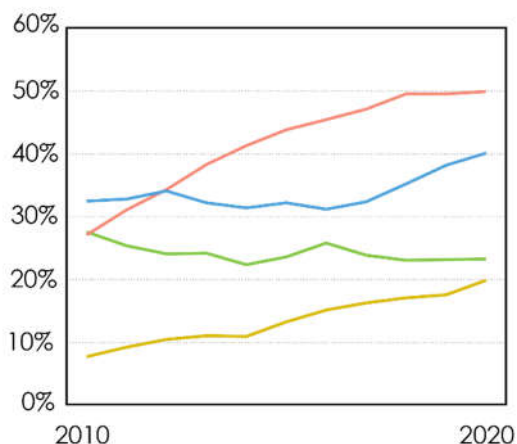
This data shows that housing supply is not increasing in any significant way, especially when compared to the 1970s and 1980s. This along with low vacancy rates across Vermont at just over 3%, and increasing costs for homes and rental units, means that would-be homeowners and renters are struggling to find adequate housing. This creates an economic burden on residents struggling to live in Hartford.

Demographics and Housing

The physical housing itself is not the only important factor when determining housing need. Understanding the population, the age of residents, size of households, and current trends can indicate what types of homes are needed and whether the current housing stock meets the needs of residents. The chart on the right shows the percent of housing

How are our households changing?

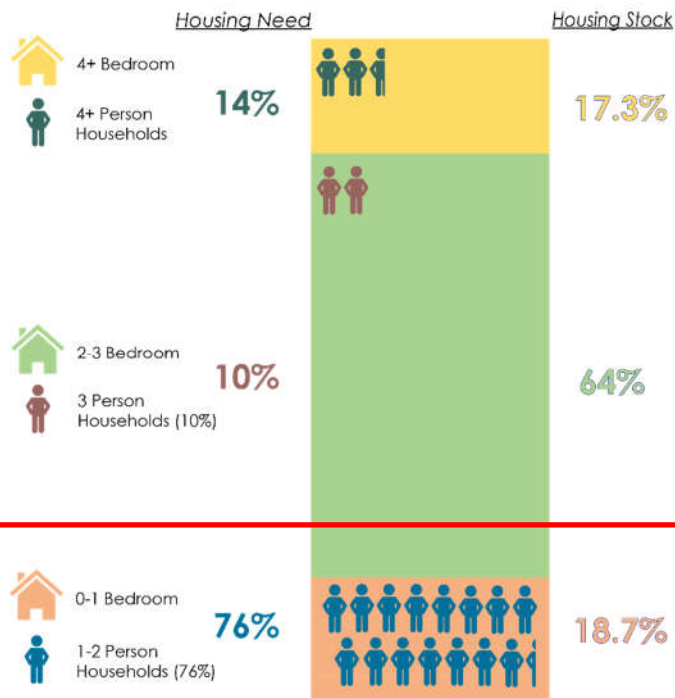
Hartford, VT



* American Community Survey
Table S1101

Does our housing stock match our household need?

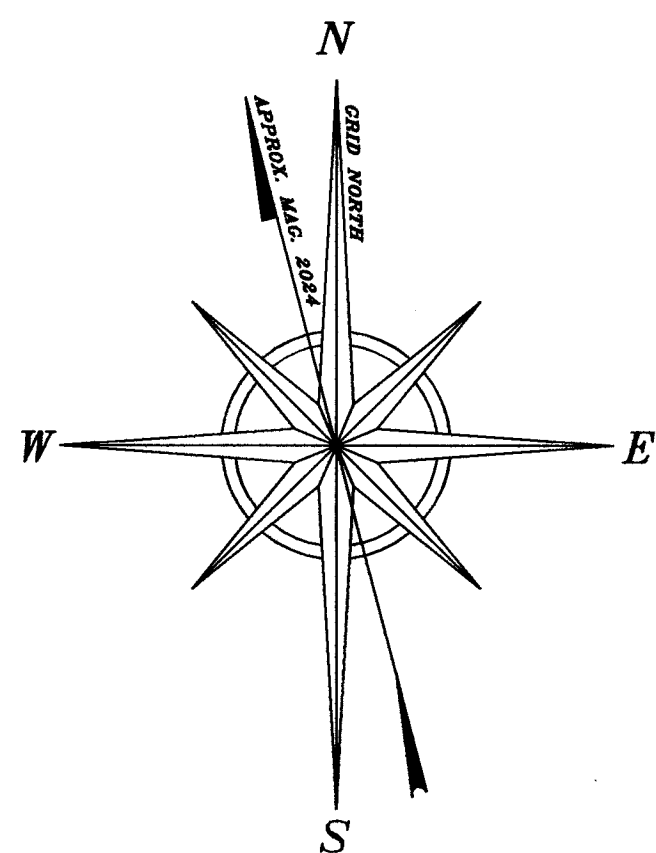
Hartford, VT



American Community Survey, 2020
Tables S2504, S2501

units by size (bar graph), while the people indicate the percent of households that would likely fit into each home size category. While there is likely some overlap between the 1-2 person households and the 2–3-bedroom homes, there is clear indication that most households are quite small, and that the housing units available are larger than what is needed or desired, especially for small families looking to buy a home for the first time, or for older adults who may be looking to downsize. Many more adequate starter homes, along with smaller units near public transit and public services, would help ensure the needs of these groups are met.

Changes in households over time can also provide some insight into what our families will look like in the future. The graph on the left shows the percentage of specific household types in Town. It is estimated that nearly 50% of households have one or more people over the age of 60. This is up from just under 30% in 2010. Other crucial indicators, householders living alone regardless of age and those over 65 who are living alone, have both increased by about 10% since 2010. Finally, households with children have declined marginally during that same period. This



ROBERT RODGERS
DEBORAH RODGERS
256 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-71

FRANK M KLYMN
JESSE J KLYMN
P.O. BOX 572
WILDER, VT 05089-0572
PARCEL ID: 9-70

MARC OKRANT
JODI OKRANT
218 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-69

SUSAN ANN GOODWIN REVOCABLE TRUST
196 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-68

BRIAN M. JORDAN
172 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001-9252
PARCEL ID: 9-67

HEERMANS FAMILY REVOCABLE TRUST
143 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-73

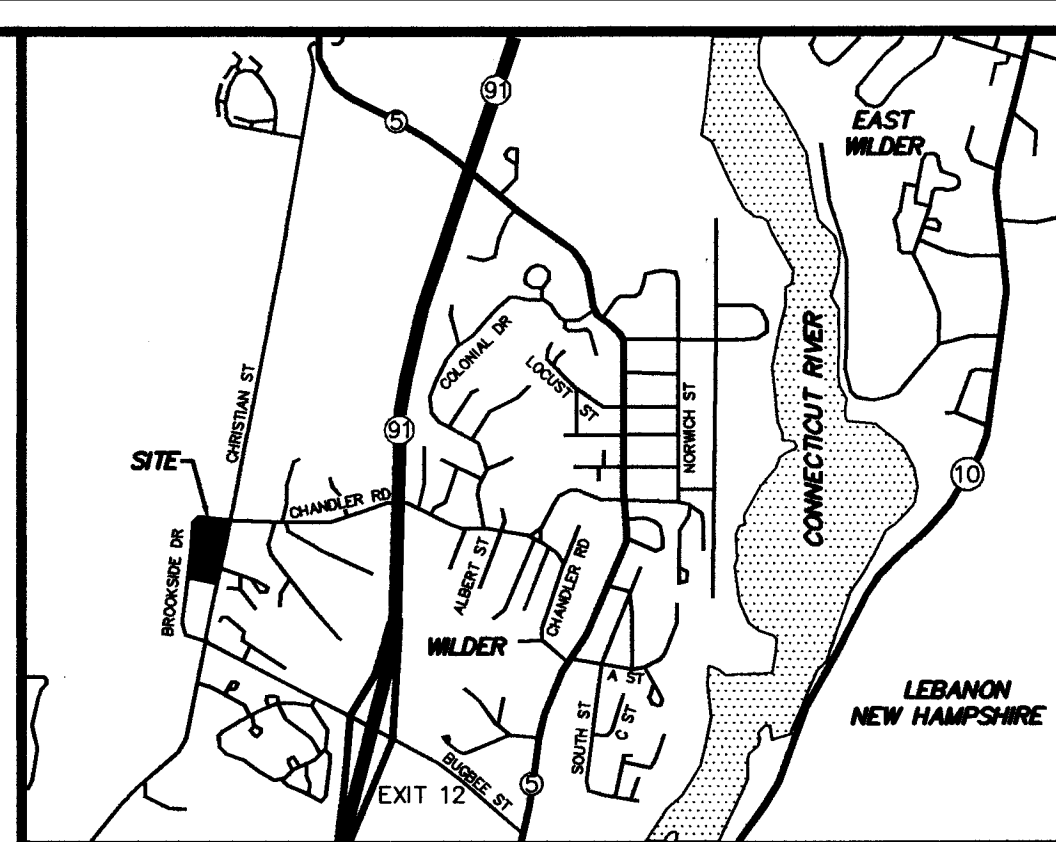
WENDELL ARTHUR BARWOOD TRUST
JUDEEN CAMERON BARWOOD TRUST
1362 CHRISTIAN STREET
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-63

SCOTT A. DAVIS
BRENDA A. CARTER
897 CHANDLER ROAD
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-63-2

JEFFREY NELSON ARNOLD
NOREEN SARDAR ARNOLD
1187 CHRISTIAN STREET
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-79

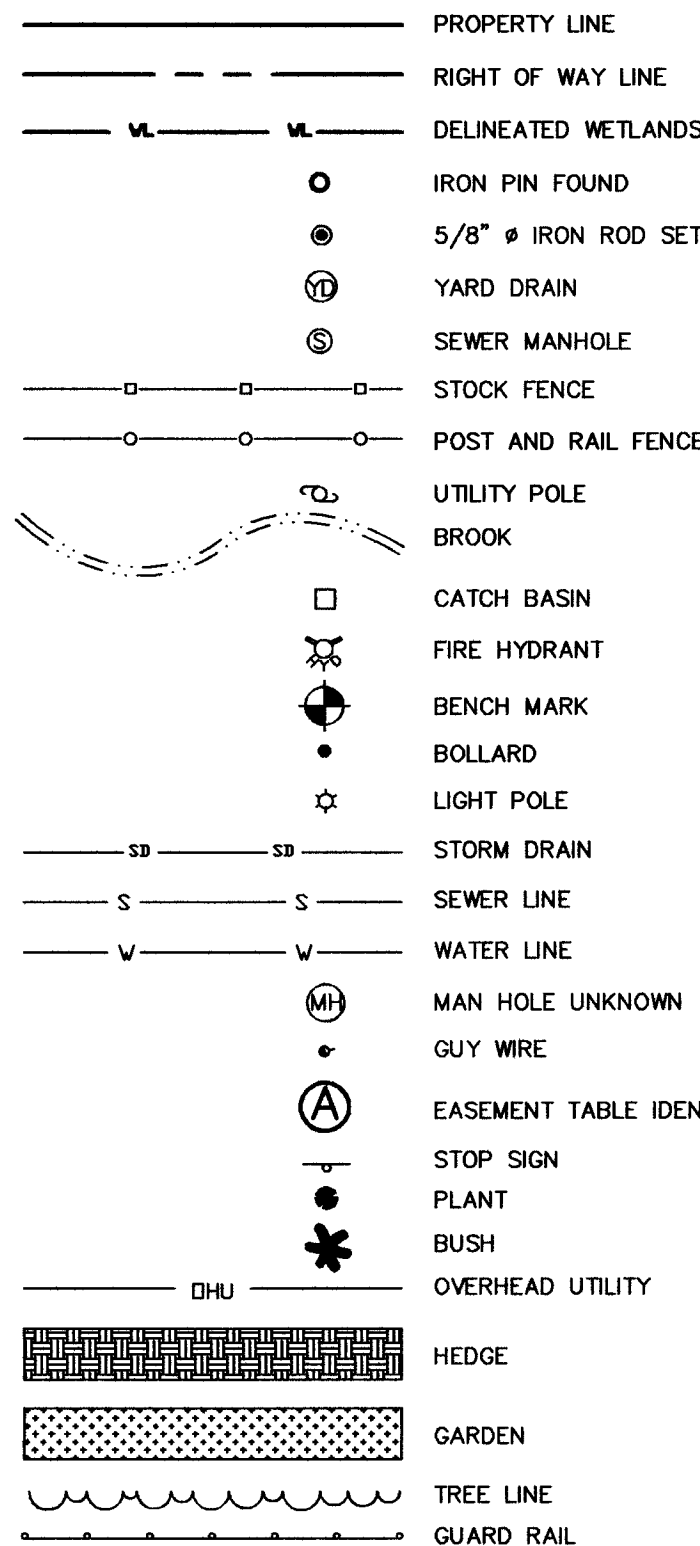
NANCY S. ARNOLD
227 NORFORD LAKE ROAD
NORWICH, VT 05055
PARCEL ID: 9-78

QUECHEE MAIN II, LLC
50 FRANKLIN STREET SUITE 400
BOSTON, MA 02110
PARCEL ID: 9-74



~ LOCATION MAP ~

~ LEGEND ~



THIS PLAT IS APPROVED BY THE PLANNING COMMISSION SUBJECT TO THE FINDINGS OF FACT, CONCLUSIONS OF LAW AND DECISION FOR THIS PLANNED DEVELOPMENT, A COPY OF WHICH IS ON FILE IN THE DEPARTMENT OF PLANNING AND DEVELOPMENT SERVICES.

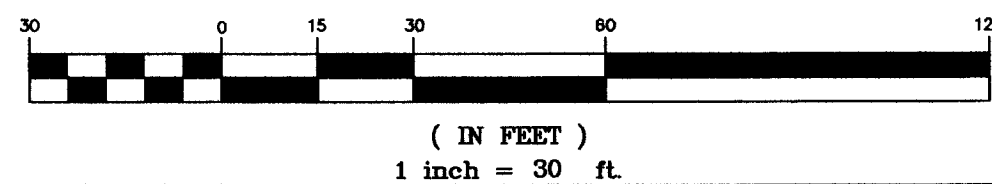
APPROVAL NO. _____ HARTFORD PLANNING COMMISSION

DATE: _____ BY: _____
CHAIRPERSON

~ REVISIONS ~

NO.	DATE	DESCRIPTION	BY
1.	7-3-24	ADDED PC SIGNATURE BLOCK AND RECORDING BLOCK.	JAD

GRAPHIC SCALE



TOWN CLERK'S OFFICE
RECEIVED FOR RECORDING

DATE: _____

AT _____ HR _____ MIN AM/PM

MAP SLIDE: _____

ATTEST: _____

~ CERTIFICATION ~

IN MY OPINION AND TO THE BEST OF MY KNOWLEDGE THIS SURVEY AND PLAN IS CONSISTENT WITH INFORMATION ABSTRACTED FROM THE TOWN OF HARTFORD LAND RECORDS AND PHYSICAL EVIDENCE FOUND IN THE FIELD.

THIS PLAT IS AN EXPRESSION OF PROFESSIONAL OPINION REGARDING THE EVIDENCE SHOWN, AND DOES NOT REPRESENT A WARRANTY OR A GUARANTEE.

JOSEPH A. DIBERNARDO VT LLS. NO. 715

12 TREMONT STREET, LLC

77 BANK STREET
LEBANON, NH 03768
PARCEL ID: 9-72

~ BROOK SIDE DRIVE & CHRISTIAN STREET ~
~ WINDSOR COUNTY ~ VILLAGE OF WILDER ~ TOWN OF HARTFORD, VERMONT ~

DIBERNARDO ASSOCIATES, LLC
VT NH
LICENSED LAND SURVEYORS

P.O. BOX 52
BELLOWS FALLS, VT 05601
802-483-3031 ~ 802-988-5609

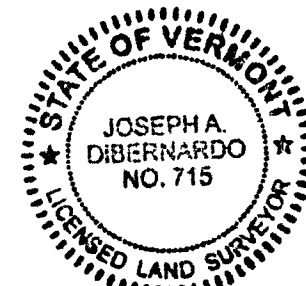
DATE MAY 16, 2024 SCALE 1"=30'

DRAWN BY JW CK'D BY JAD

SURVEYED BY BG, JD

DWG. NO. 3577

© COPYRIGHT 2024 DIBERNARDO ASSOCIATES, LLC.



S:\PROJECTS\2024\24085 BROOKSIDE APARTMENTS\DWGSHEET FILES\24085 BROOKSIDE C1.0 DEMOLITION PLAN.DWG
PLOTTED: 7/8/2024 9:20 AM



ANN GOODWIN REVOCABLE TRUST
196 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-68

MARC OKRANT
JODI OKRANT
218 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-69

SMH 4
RIM EL: 554.26'
8" PVC IN EL: 544.29'
8" PVC OUT EL: 544.25'

SMH 5
RIM EL: 549.51'
8" PVC IN EL: 542.80'
8" PVC IN EL: 542.81'
10" PVC OUT EL: 542.69'

CB 4
RIM EL: 556.65'
15" CMP OUT EL: 553.69'

SMH 3
RIM EL: 557.37'
8" PVC IN EL: 546.56'
8" PVC IN EL: 546.53'
8" PVC OUT EL: 546.39'

CB 3
RIM EL: 556.59'
15" CMP IN EL: 553.14'
15" CMP OUT EL: 553.13'

FRANK M KLYMN
JESSE J. KLYMN
P.O. BOX 572
WILDER, VT 05088-0572
PARCEL ID: 9-70

SMH 2
RIM EL: 553.52'
8" PVC OUT EL: 548.54'

CB 2
RIM EL: 554.61'
15" CMP IN EL: 551.71'
15" CMP OUT EL: 551.60'

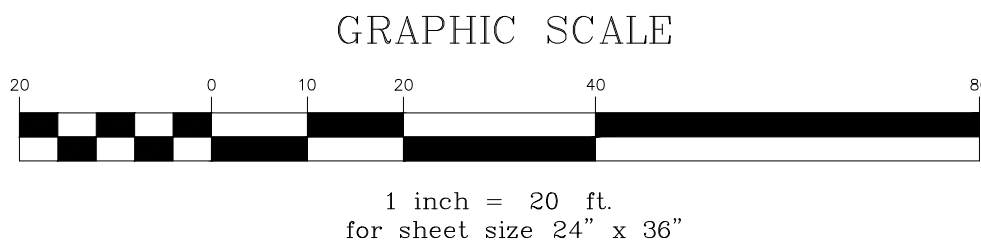
CB1
RIM EL: 552.16'
15" CMP IN EL: 548.81'
15" CMP OUT EL: 548.68'

ROBERT RODGERS
DEBORAH RODGERS
256 BROOKSIDE DRIVE
WHITE RIVER JCT, VT 05001
PARCEL ID: 9-71



DEMOLITION NOTES

- X1 REMOVE OIL TANK, PIPING AND PUMP HOUSE
- X2 REMOVE PAVEMENT
- X3 REMOVE LPG TANKS
- X4 REMOVE LIGHT
- X5 REMOVE PIPE



Stamp	
Date	Description

ENGINEERING VENTURES PC	208 Flynn Avenue, Suite 2A, Burlington, VT 05401 • 802-863-6225
	85 Mechanic Street, Suite E2-3, Lebanon, NH 03766 • 603-442-3333
12 Tremont Street, LLC	77 Bank Street
	Lebanon, New Hampshire 603-727-9024

Demolition Plan	Brookside Apartments
	Hartford, Windsor County, Vermont

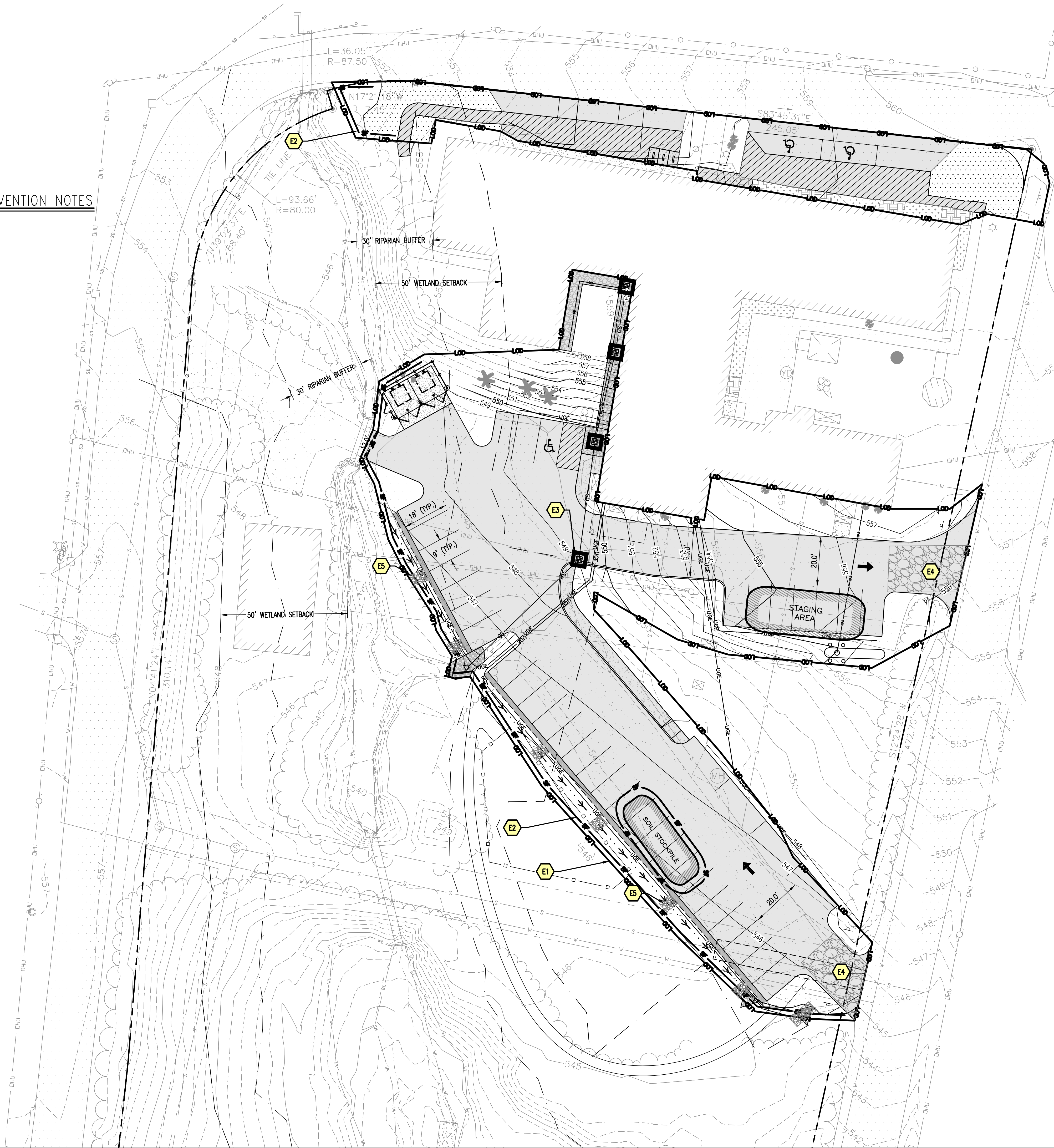
EV Project #	24085
Drawn By:	AM
Checked By:	AM
Scale:	1" = 20'
Date:	7/8/2024

C1.0



STORMWATER POLUTION PREVENTION NOTES

- E1 LIMIT OF DISTURBANCE
- E2 SILT FENCE
- E3 INLET PROTECTION (TYP.)
- E4 STABILIZED CONSTRUCTION ENTRANCE
- E5 STONE CHECK DAM (TYP.)



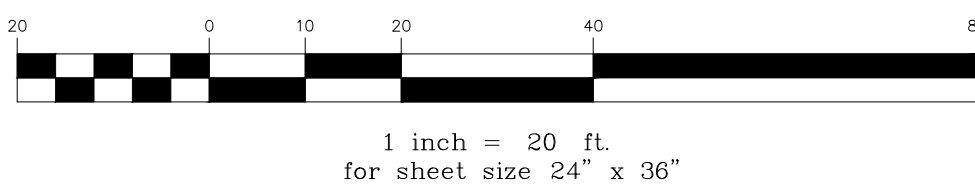
CONSTRUCTION SEQUENCE

1. CONTRACTOR TO COORDINATE STOCKPILE AND STAGING AREAS WITH THE OWNER.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL HOST A PRECONSTRUCTION MEETING WITH OWNER, ENGINEER, PLANNING DEPARTMENT, POLICE, FIRE, AND PUBLIC WORKS STAFF.
3. PRIOR TO ANY OTHER WORK, SILT FENCES SHALL BE INSTALLED GENERALLY 10 FEET FROM THE BASE OF THE FILL SLOPES, OR AS SHOWN ON THE EROSION CONTROL PLAN. THESE SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN STABILIZED. SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE WHEN IT BECOMES 6 INCHES DEEP AT THE FENCE. THE SILT FENCE WILL BE REPAIRED AS NECESSARY TO MAINTAIN A PROPER SEDIMENT BARRIER.
4. INSTALL TEMPORARY GRAVEL CONSTRUCTION ENTRANCE/EXIT.
5. INSTALL INLET PROTECTION ON EXISTING CATCH BASINS AFFECTED DURING CONSTRUCTION.
6. ROUGH GRADE AND STOCKPILE TOPSOIL SURROUNDED BY SILT FENCE.
7. DEMO EXISTING STRUCTURES AS REQUIRED.
8. CLEAR AND GRUB, REMOVE LOOSE PAVING/DEMO PAVING AS REQUIRED AND HAUL OFF SITE USING EXISTING PARKING AREA AS STAGING.
9. GRADE DRIVE ENTRANCE AND EXCAVATE BUILDING PAD.
10. BEGIN VERTICAL CONSTRUCTION OF BUILDING ADDITION CONCURRENT WITH WATER AND SEWER UTILITIES.
11. CONSTRUCT STORMWATER SYSTEM.
12. INSTALL STORMWATER PIPING AND STRUCTURES.
13. BACKFILL AND SUBGRADE PREP FOR NEW PARKING AND SIDEWALK AREAS.
14. COMPLETE FINAL GRADING OF SITE, PLACE TOPSOIL AND PERMANENTLY VEGETATE, LANDSCAPE, AND MULCH.
15. INSTALL LIGHT POLES.
16. PAVE BASE COURSE.
17. STABILIZE EXPOSED SLOPES AND SOILS AS SOON AS GRADED, REPAIR AND REESTABLISH ALL DISTURBED VEGETATED/LAWN AREAS, AND MAINTAIN UNTIL ADEQUATELY VEGETATED.
18. PAVE WEARING COURSE.
19. AFTER THE SITE IS APPROVED BY THE ENGINEER AS ADEQUATELY STABILIZED, REMOVE ALL TEMPORARY.
20. ANTICIPATED FINAL COMPLETION DATE SUMMER/FALL 2024.

EROSION CONTROL LEGEND

- EROSION PREVENTION AND SEDIMENT CONTROL STRATEGY**
THE FOLLOWING TECHNIQUES WILL BE UTILIZED AS PART OF A SEDIMENT AND EROSION CONTROL PROGRAM. THE SEDIMENT AND EROSION CONTROL PROGRAM WILL BE IMPLEMENTED IN STAGES. CERTAIN ITEMS FROM ONE STAGE WILL LIKELY OVERLAP OR TAKE PLACE CONCURRENTLY WITH ITEMS FROM OTHER STAGES. HAY PRODUCTS SHALL NOT BE USED.
- TEMPORARY INLET PROTECTION** - STONE COLLARS AND/OR INLET SOCKS. INSTALL AS INDICATED ON PLANS. STONE TO BE REMOVED AND REPLACED WITH CLEAN STONE WHEN SEDIMENT IS $\leq 1/2$ DEPTH OF STONE. REMOVE ALL SEDIMENT COLLECTED IN STRUCTURE OR SOCK AS SOON AS POSSIBLE.
- STONE CHECK DAM**
THIS STRUCTURAL MEASURE IS ALONG DITCHES/ SWALES AND IN OTHER LOCATIONS INDICATED ON THIS PLAN TO SLOW STORMWATER RUNOFF AND TRAP SEDIMENT PARTICLES. THESE WILL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN PERMANENTLY STABILIZED.
- PERMANENT EROSION CONTROL NETTING**
THIS STRUCTURAL MEASURE IS INSTALLED IN AREAS THAT HAVE SLOPES 3:1 AND GREATER AND IN OTHER LOCATIONS INDICATED ON THIS PLAN TO STABILIZE THE SLOPE AND REDUCE THE EROSION POTENTIAL. THE NETTING IS TYPICALLY IMPREGNATED WITH GRASS SEED AND SOMETIMES STAPLED TO THE EXPOSED SOIL. THESE WILL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN PERMANENTLY STABILIZED.
- TEMPORARY STABILIZED CONSTRUCTION ENTRANCE**
THIS STRUCTURAL MEASURE IS A STABILIZED PAD OF AGGREGATE UNDERLAIN WITH FILTER FABRIC LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY, SIDEWALK, OR PARKING AREA. THE PURPOSE OF A STABILIZED CONSTRUCTION ENTRANCE IS TO REDUCE OR ELIMINATE THE TRACKING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY OR STREETS. THIS WILL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN PERMANENTLY STABILIZED. ONCE REMOVED, THE IMPACTED AREA SHALL BE SEEDED AND MULCHED.
- TEMPORARY SEDIMENT BASIN**
THIS STRUCTURAL MEASURE INVOLVES CONSTRUCTING TEMPORARY SEDIMENT BASINS IN THE LOCATION OF PROPOSED STORMWATER PONDS. GENERALLY THE STORMWATER PONDS CAN BE CONSTRUCTED AS SHOWN ON THE PLANS HOWEVER ORIFICE ON THE OUTLET STRUCTURES SHALL BE OMITTED UNTIL THE SITE IS STABILIZED TO IMPROVE RUNOFF AND ALLOW FOR SETTLING OF SUSPENDED FINES. THIS WILL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN PERMANENTLY STABILIZED. ONCE THE SITE IS STABILIZED, SEDIMENT SHALL BE REMOVED FROM THE BASINS AND THE OUTLET ORIFICE SHALL BE DRILLED INTO THE OUTLET STRUCTURE AS NOTED ON THE PLANS.
- TEMPORARY STAGING AND WASTE AREAS (APPROXIMATE)**
THESE ARE APPROVED LOCATIONS WHERE NON-SOIL, NON-ERODIBLE MATERIALS MAY BE STORED. SOILS SHALL NOT BE STORED IN THESE AREAS.
- TEMPORARY SOIL STOCKPILE AREAS (APPROXIMATE)**
THESE ARE APPROVED LOCATIONS WHERE TOPSOIL AND OTHER SOIL MATERIALS MAY BE STORED. THESE STOCKPILES WILL BE PROTECTED FROM EROSION BY A NUMBER OF METHODS, INCLUDING INSTALLING SILT FENCING AROUND THE DOWN GRADIENT PERIMETER OF THE STOCKPILE AND SEEDING AND MULCHING THE STOCKPILE WHEN NOT IN USE FOR MORE THAN FIVE DAYS.
- TEMPORARY SILT FENCING**
THIS STRUCTURAL MEASURE IS A TEMPORARY BARRIER OF GEOTEXTILE FABRIC USED TO INTERCEPT SEDIMENT LAKE RUNOFF FROM SMALL DRAINAGE AREAS OF DISTURBED SOIL. IT IS INSTALLED ALONG THE PERIMETER OF IMPACTED AREAS AND ALONG THE BASE OF THE FILL SLOPES. ADDITIONALLY, WHEN DESIGNATED ALONG THE LIMITS OF DISTURBANCE, INSTALL CONSTRUCTION FENCE BEHIND THE SILT FENCE. SILT FENCING IS EFFECTIVE IN REDUCING STORMWATER RUNOFF VELOCITIES, ASSIST IN THE DEPOSITION OF TRANSPORTED SEDIMENT LOAD AND PREVENT EROSION OF SOILS ONTO ADJACENT AREAS. THESE WILL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN PERMANENTLY STABILIZED.
- LIMITS OF DISTURBANCE**
THE CONTRACTOR SHALL CONTAIN ANY EARTH MOVING ACTIVITIES WITHIN THE DESIGNATED LIMITS SHOWN ON THIS PLAN. THE ENGINEER SHALL REVIEW THE SITE TO MAKE ANY ADJUSTMENTS TO ACCOUNT FOR ENVIRONMENTALLY SENSITIVE AREAS, SPECIMEN TREES AND SPECIAL AREAS OF CONCERN.

GRAPHIC SCALE



Stamp

Date

Description

No.

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Erosion Prevention and Sediment Control Plan

Brookside Apartments
Hartford, Windsor County, Vermont

Sheet Title:

EV Project # 24086

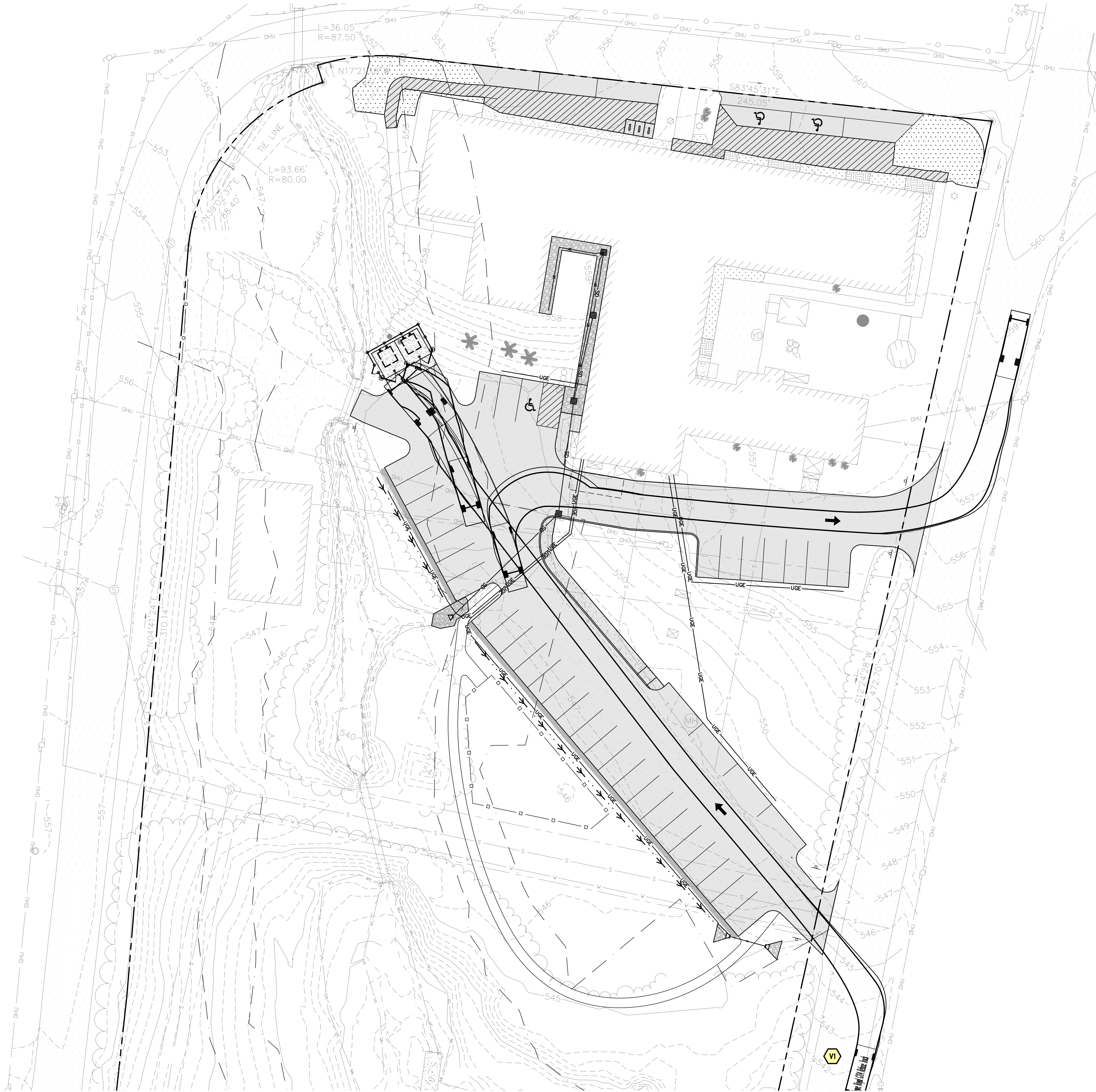
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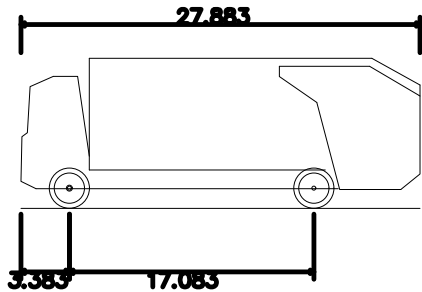
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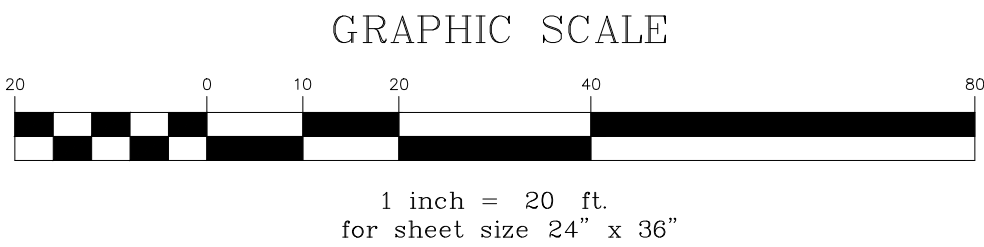


VEHICLE TURNING NOTES

V1 GARBAGE TRUCK



Hino 338 M + Wayne Royal GT14 Refuse Truck
Overall Length 27.883ft
Overall Width 8.042ft
Overall Body Height 10.488ft
Min Body Ground Clearance 1.318ft
Track Width 8.042ft
Lock-to-lock time 9.00s
Curb to curb Turning Radius 27.400ft



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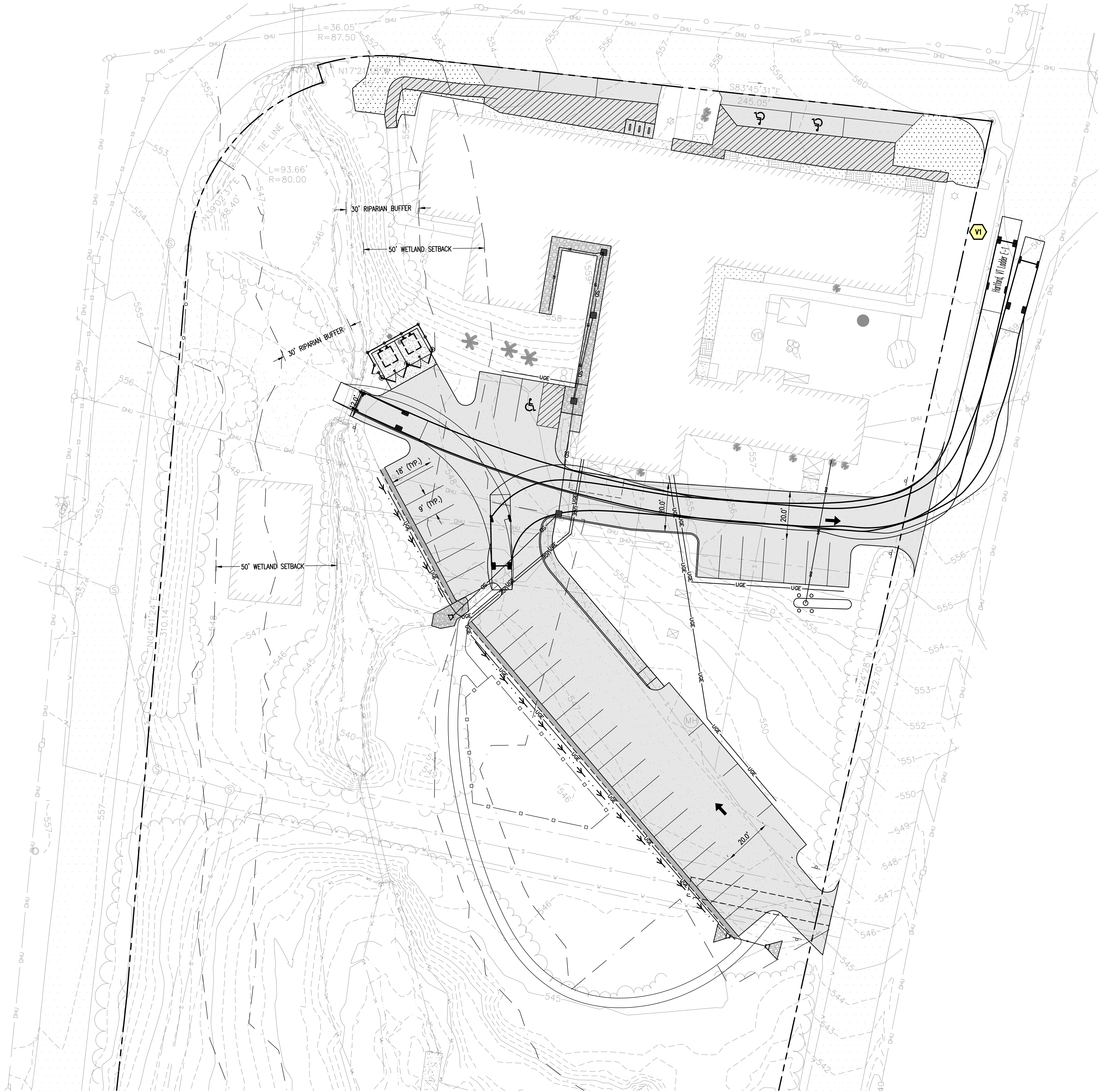
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Garbage Truck Turning Plan

Brookside Apartments
Hartford, Windsor County, Vermont

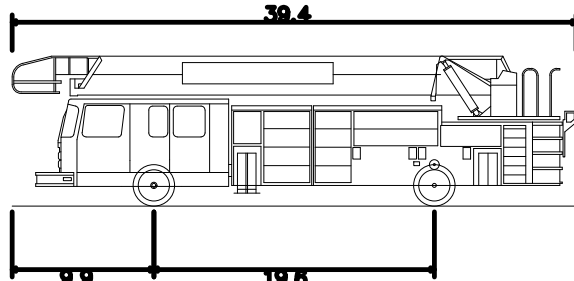
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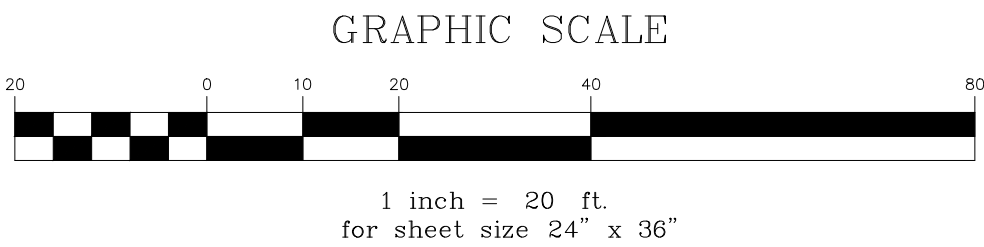


VEHICLE TURNING NOTES

V1 HARTFORD LADDER TRUCK



Hartford, VT Ladder E-1
Overall Length 39.400ft
Overall Width 8.300ft
Overall Body Height 10.484ft
Min. Body Ground Clearance 0.915ft
Track Width 8.300ft
Lock-to-lock time 4.00s
Curb to Curb Turning Radius 35.100ft



Stamp	
Date	
Description	
No.	

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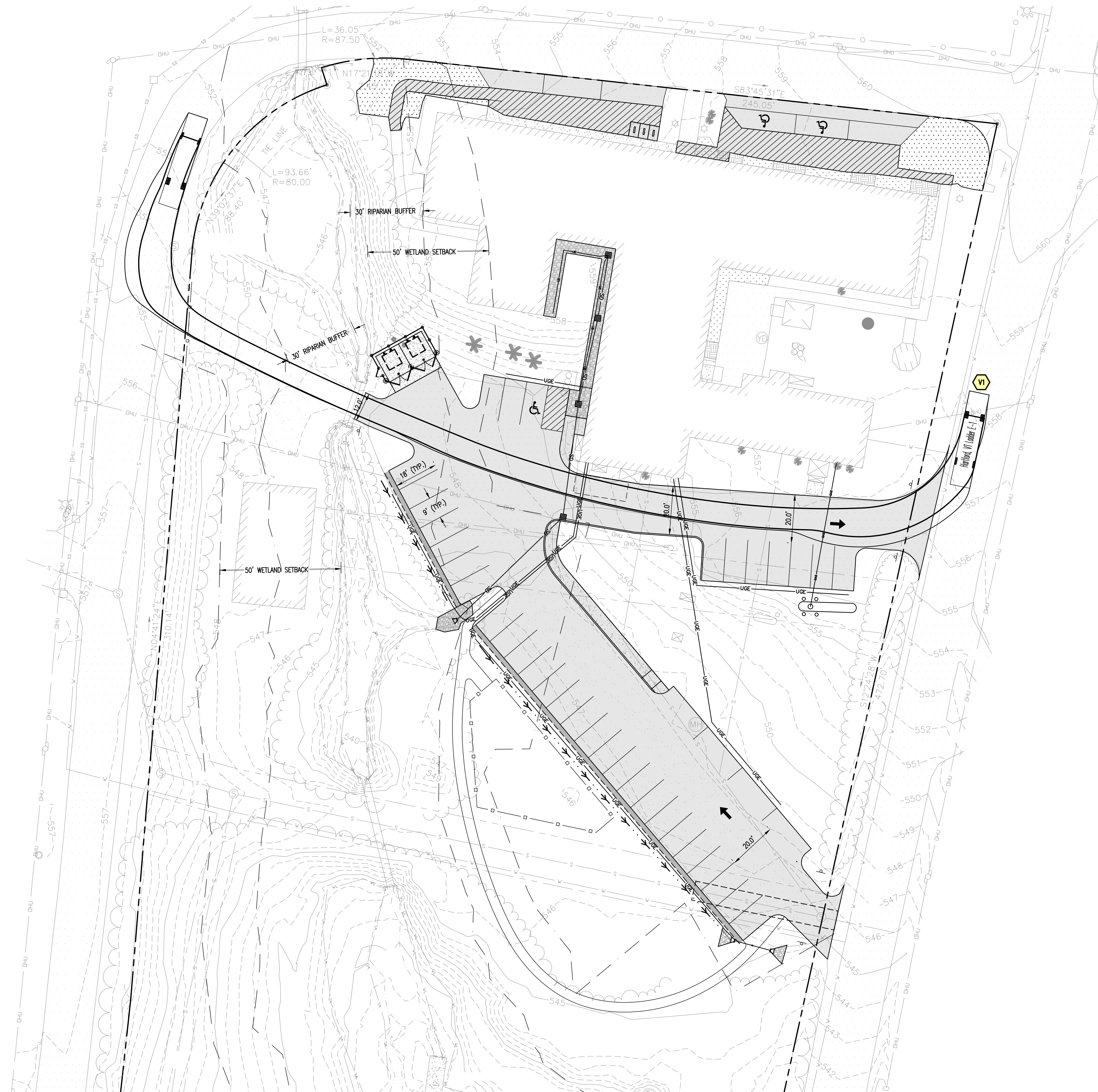
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603-727-9024

Fire Truck Turning Plan

Brookside Apartments
Hartford, Windsor County, Vermont

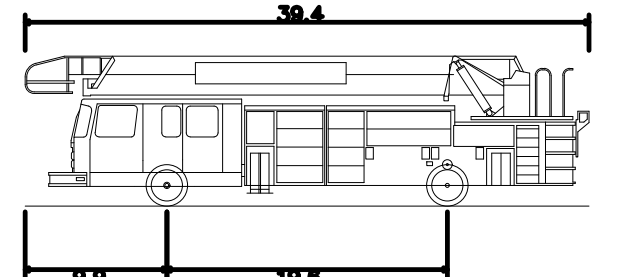
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Checked By: AM
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Date: 7/8/2024

C1.3

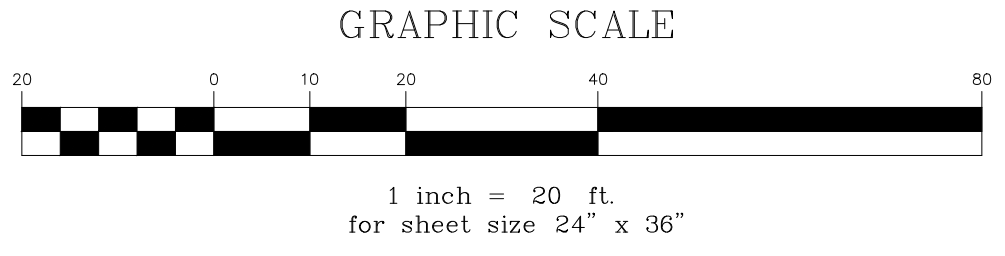


VEHICLE TURNING NOTES

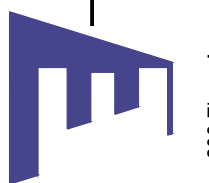
V1 HARTFORD LADDER TRUCK



Hartford, VT Ladder E-1
Overall Length 39.400ft
Overall Width 8.300ft
Overall Body Height 10.484ft
Min. Body Ground Clearance 0.915ft
Track Width 8.300ft
Lock-to-lock time 4.00s
Curb to Curb Turning Radius 35.100ft



Stamp	
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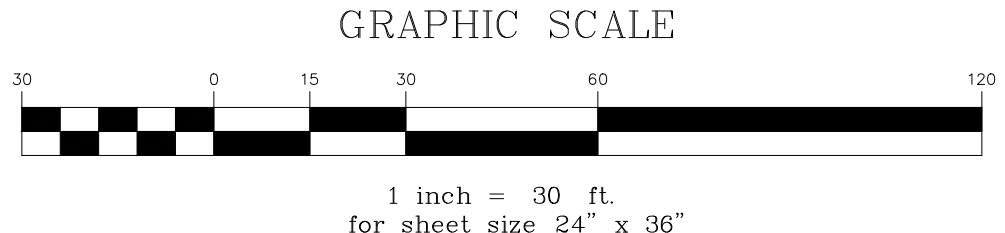
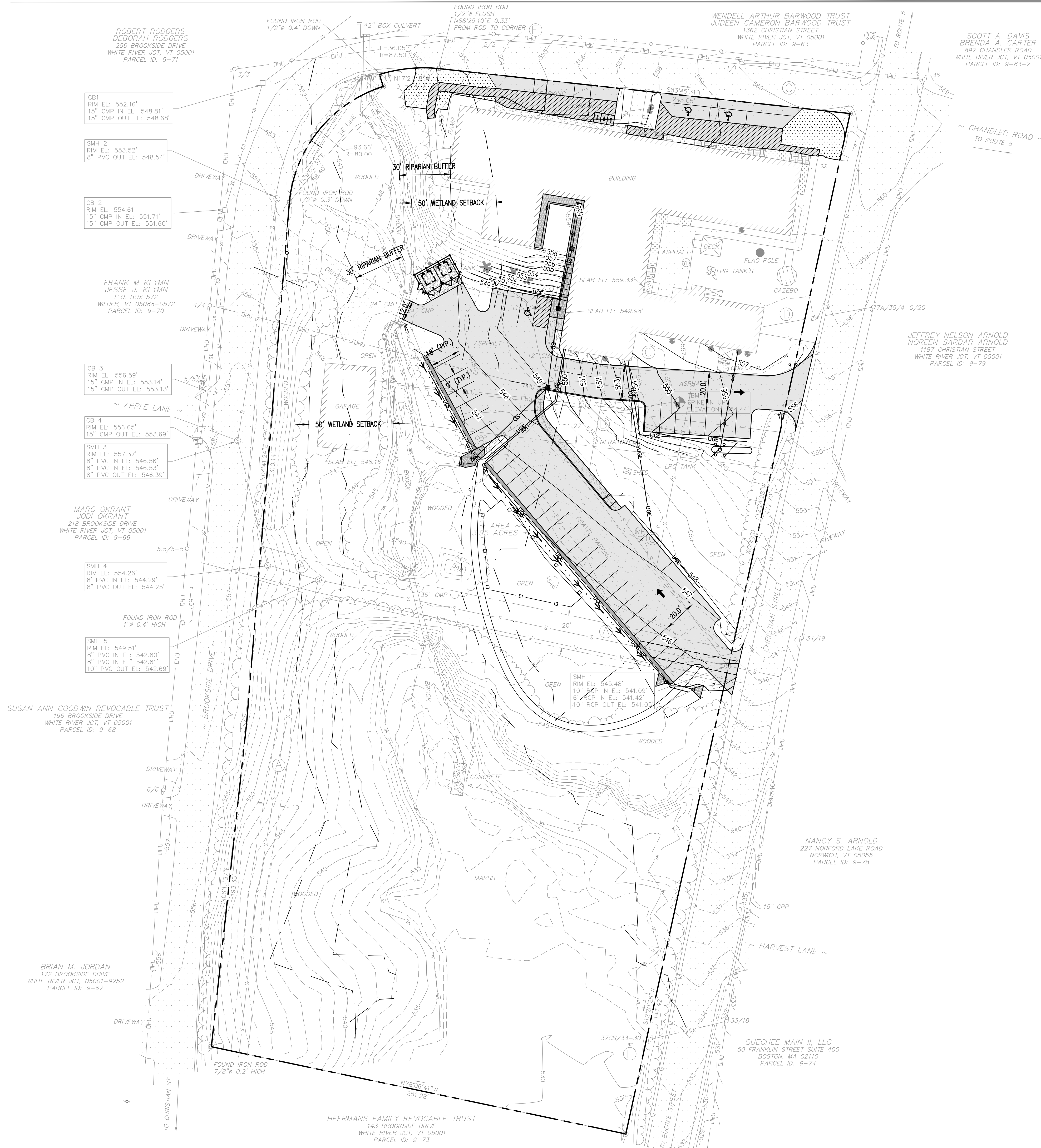
Fire Truck Turning Plan (2)

Brookside Apartments
Hartford, Windsor County, Vermont

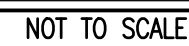
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C1.4

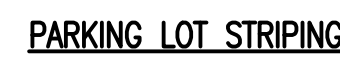
S:\PROJECTS\2024\24085 BROOKSIDE APARTMENTS\DWG\SHEET FILES\24085 BROOKSIDE C2.0 SITE PLAN.DWG PLOTTED: 7/8/2024 9:18 AM



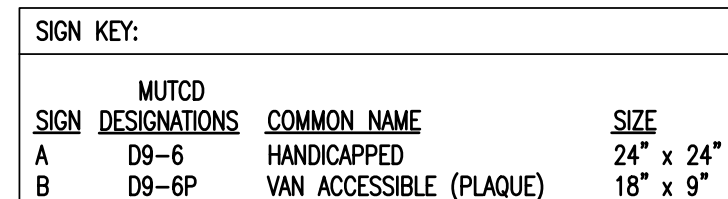
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12 Tremont Street, LLC 77 Bank Street Lebanon, New Hampshire 603-727-9024	
Site Plan Overall	
Brookside Apartments Hartford, Windsor County, Vermont	
EV Project #	24085
Drawn By:	AM
Checked By:	AM
Scale:	1" = 30'
Date:	7/8/2024
C2.0	



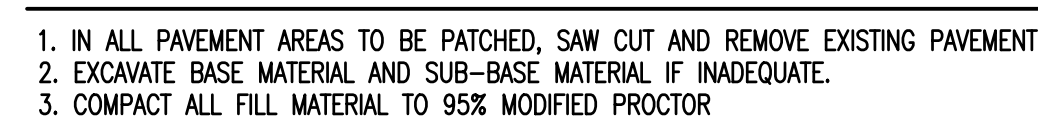
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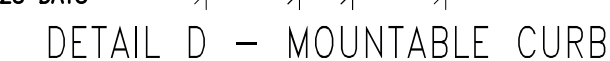
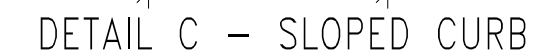


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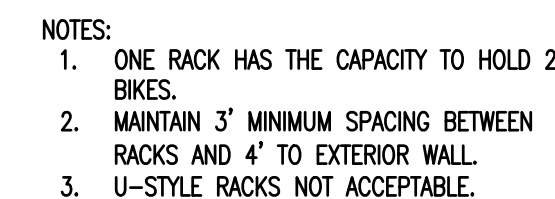
- NOTES:
1. CONCRETE TO BE 4000psi WITH 5-7% AIR ENTRAINMENT.
 2. BROOM FINISH WITH SMOOTH TROWELED EDGES. TREAT WITH SILANE-SILOXANE OR EQUIVALENT.
 3. CONTROL JOINT SHALL BE A 1/8" WIDE JOINT AT LEAST 1/3 OF THE DEPTH OF THE PAD.

NOT TO SCALE

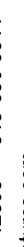
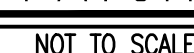


NOT TO SCALE

- VERTICAL GRANITE CURB NOTES:**
- 1. GRANITE CURBS SHALL BE INSTALLED IN ACCORDANCE WITH PROJECT AND STATE SPECIFICATIONS
 - 2. WHERE CURB IS ADJACENT TO ENTRY/EXIT DOOR PADS WITH FROST WALL FOUNDATIONS, CURB SHALL BE DOWELED TO PAD WITH 2" LONG #4 DOWELS (CENTERED) AT 1'-6" O.C.
 - 3. IF DOWEL IN PAD IS NOT TO BE GRADED.
 - 4. WHERE CURB IS ADJACENT TO SIDEWALK, BOLLARD, LIGHT BASE OR OTHER HARD FEATURES, 1/2" EXPANSION MATERIAL (FULL DEPTH OF CURB), SHALL BE INSTALLED BETWEEN FEATURE & CURB.



NOT TO SCALE

The logo for Engineering Ventures PC features a large, stylized blue letter 'E' on the left. To the right of the 'E', the words 'ENGINEERING' and 'VENTURES' are stacked vertically in a bold, black, sans-serif font. A thin horizontal line separates 'ENGINEERING' from 'VENTURES'. The letters 'PC' are positioned to the right of 'VENTURES' in a smaller, black, sans-serif font.

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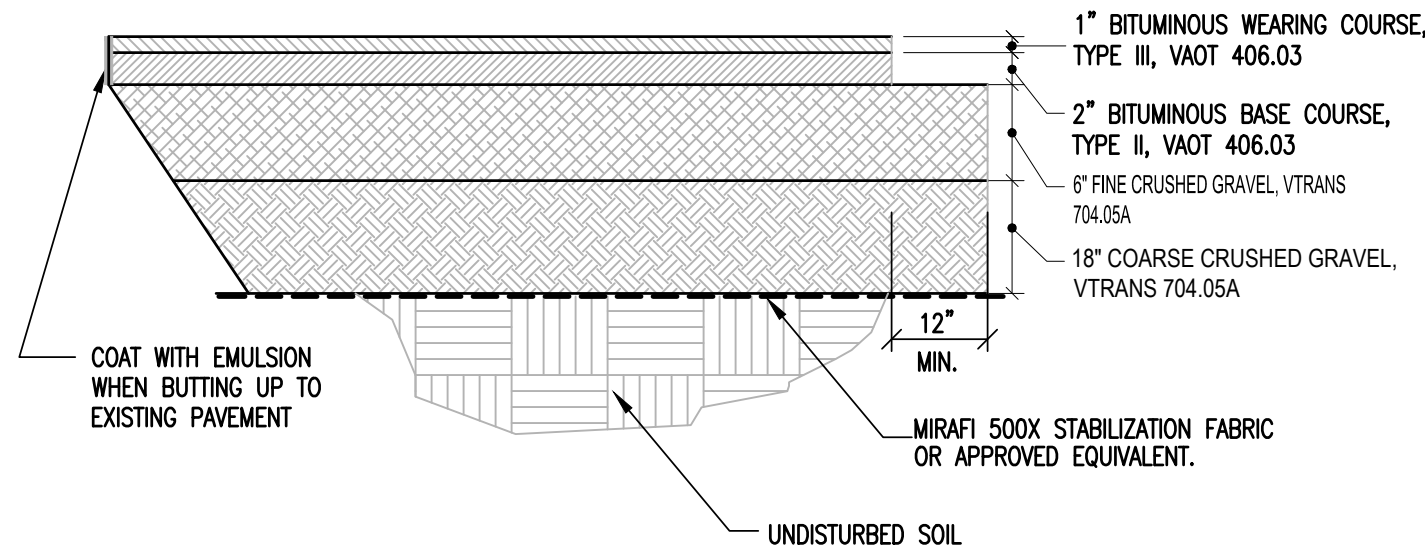
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Brookside Apartments
Hartford, Windsor County, Vermont

Site Details

Project #	24085
Drawn By:	AM
Checked By:	AM
Scale:	NTS
Date:	7/8/2024

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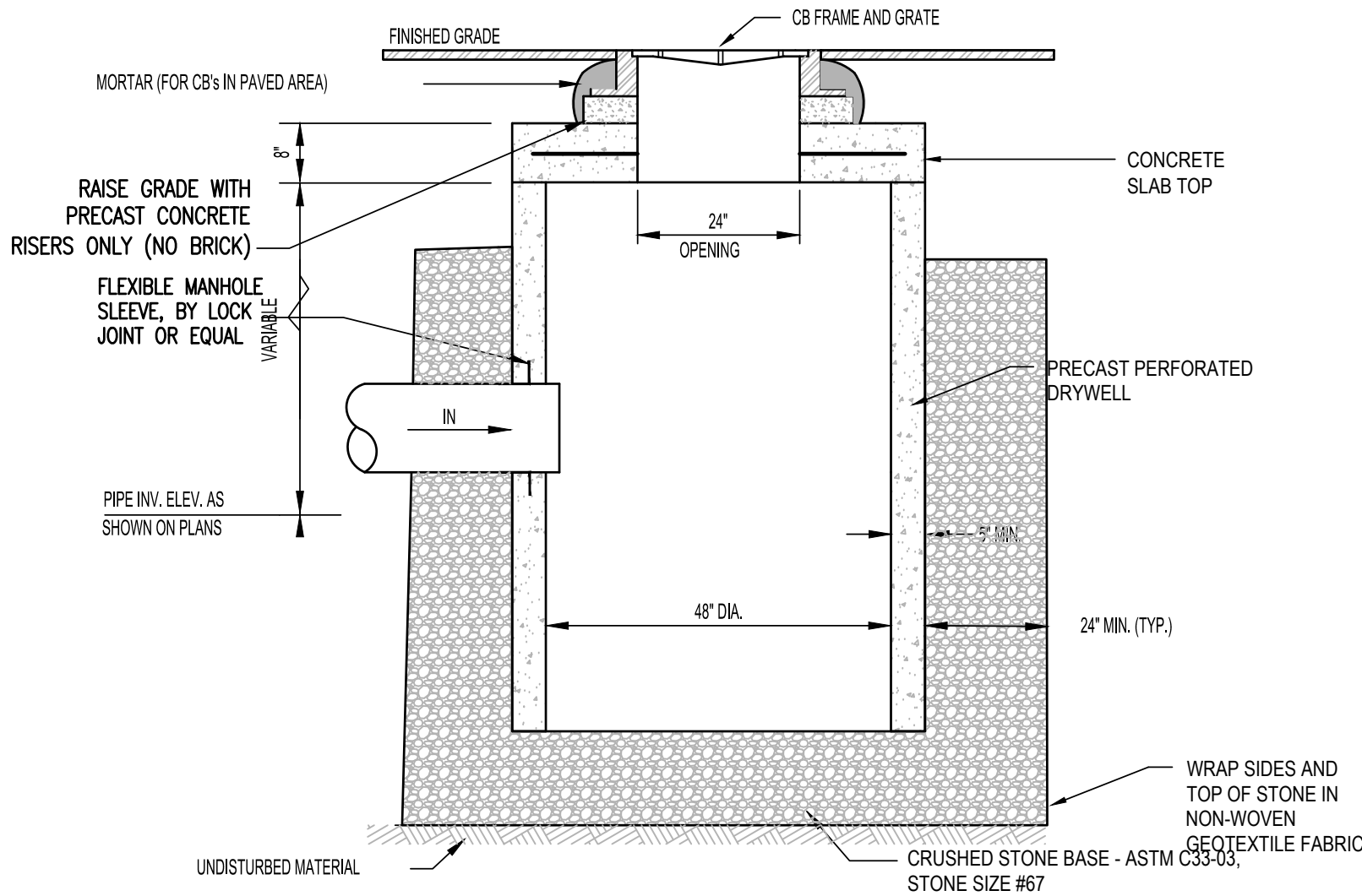


- PAVEMENT PATCHING NOTES:
1. IN ALL PAVEMENT AREAS TO BE PATCHED, SAW CUT AND REMOVE EXISTING PAVEMENT.
 2. EXCAVATE BASE MATERIAL AND SUB-BASE MATERIAL IF INADEQUATE.
 3. COMPACT ALL FILL MATERIAL TO 95% MODIFIED PROCTOR

PAVEMENT TYPICAL SECTION

NOT TO SCALE

1



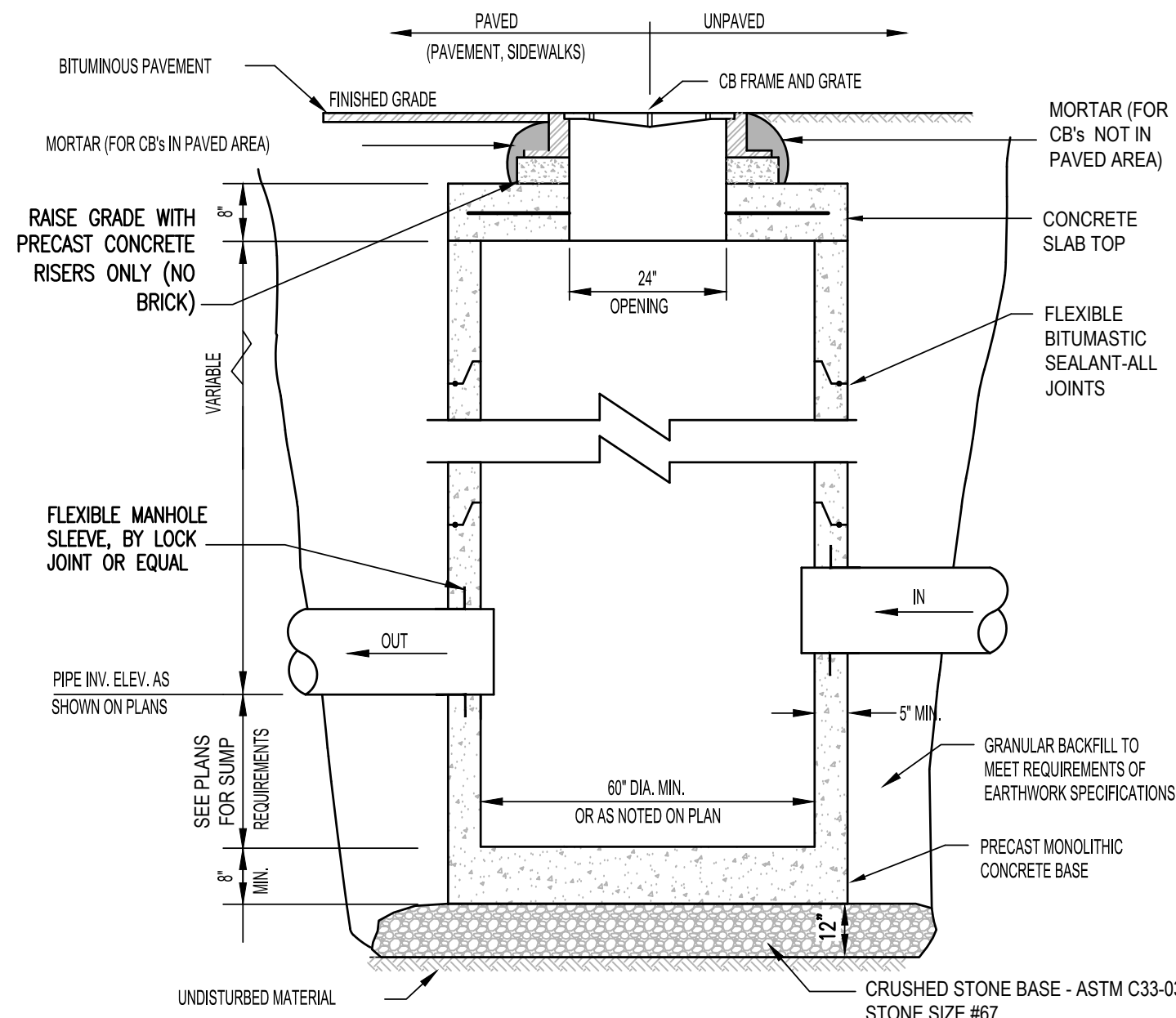
NOTES

1. IN PAVEMENT AREAS, PROVIDE A LEBARON 24"x24" TYPE E OR EQUAL, 3 FLANGE GRATE AND FRAME NEXT TO CURBS AND 4 FLANGE GRATE AND FRAME AT ALL OTHER LOCATIONS. IN GRASS AREAS, PROVIDE A 24" ROUND GRATE.
2. CATCH BASIN AND GRATE SHALL BE DESIGNED FOR H20 LOADING.

DRY WELL DETAIL

NOT TO SCALE

2



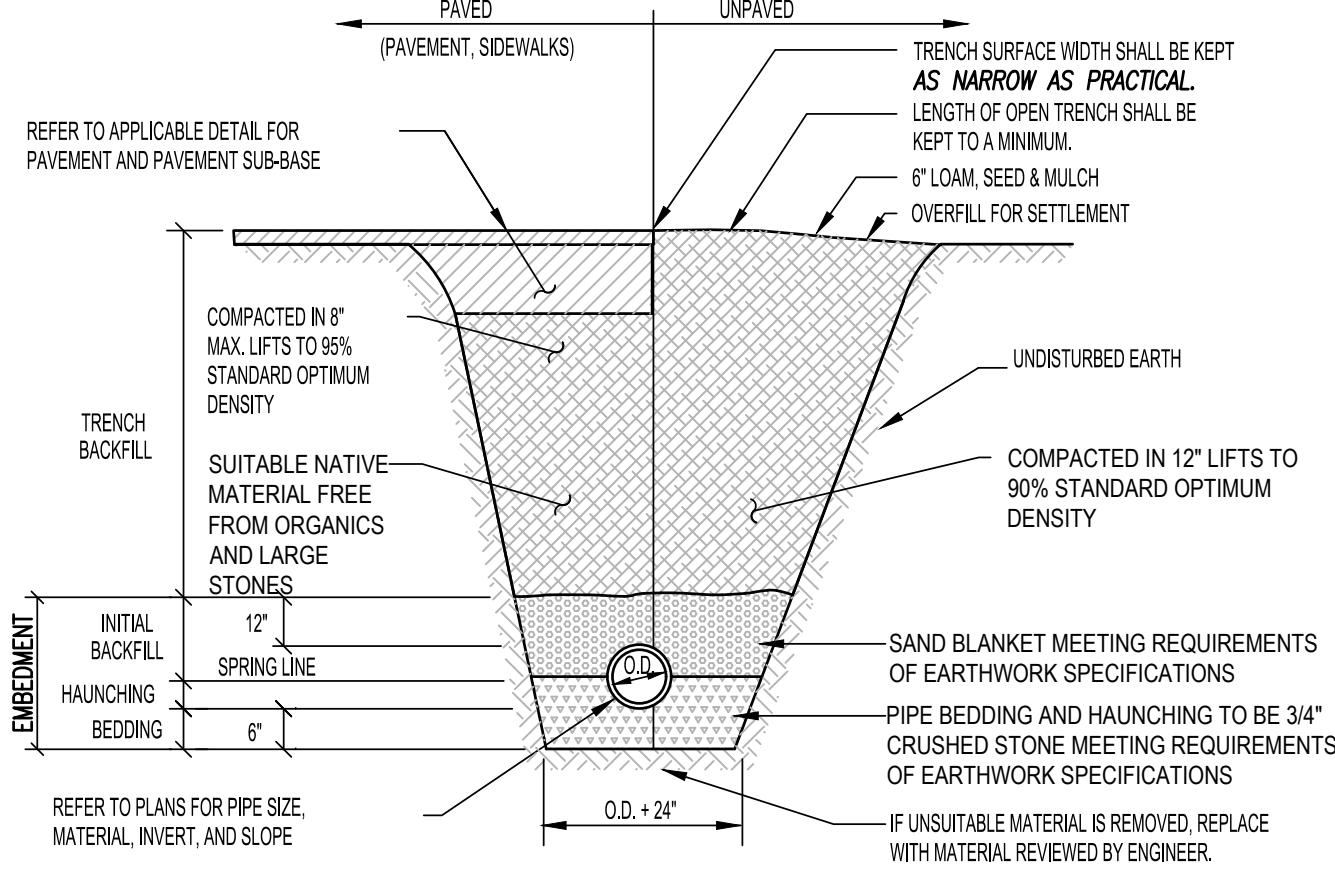
CATCH BASIN NOTES

1. IF DEPTH OF CATCH BASIN IS 7 FEET OR LESS FROM RIM TO CENTERLINE INVERT THEN A FLAT TOP WILL BE INSTALLED. IF DEPTH OF CATCH BASIN FROM RIM TO CENTERLINE INVERT IS MORE THAN 7 FEET THEN A CONICAL TOP WILL BE INSTALLED.
2. IN PAVEMENT AREAS, PROVIDE A LEBARON 24"x24" TYPE E OR EQUAL, 3 FLANGE GRATE AND FRAME NEXT TO CURBS AND 4 FLANGE GRATE AND FRAME AT ALL OTHER LOCATIONS. IN GRASS AREAS, PROVIDE A 24" ROUND GRATE.
3. CATCH BASIN AND GRATE SHALL BE DESIGNED FOR H20 LOADING.

CATCH BASIN DETAIL

NOT TO SCALE

3



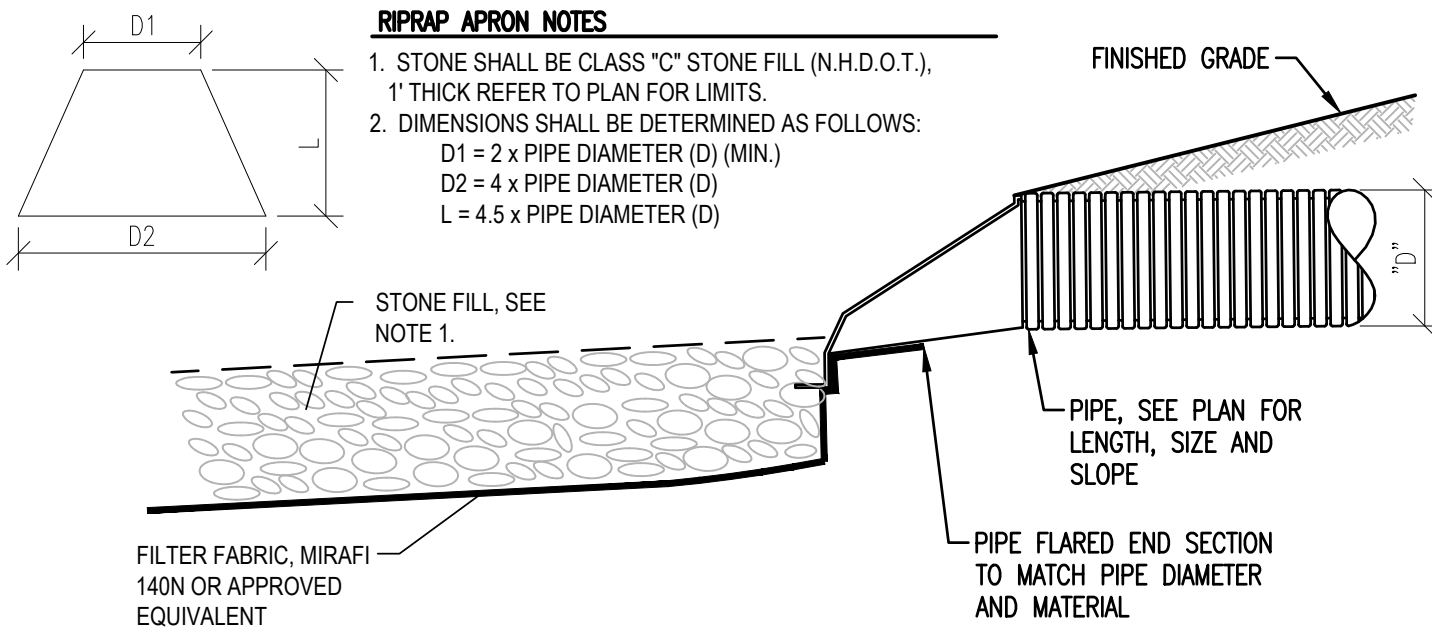
STORM DRAIN TRENCH NOTES

1. UNLESS OTHERWISE NOTED, ASSUME CLASS "C" SOILS. PERFORM ALL EXCAVATIONS TO OSHA REQUIREMENTS.
2. BEDDING TO PROVIDE A FIRM, STABLE, CONTINUOUS AND UNIFORM SUPPORT FOR THE FULL LENGTH OF PIPE.
3. WHEN APPLICABLE INSTALL PIPE WITH BELL ENDS DOWN SLOPE. PREVENT SEDIMENT FROM ENTERING NEW STORM DRAIN SYSTEM DURING CONSTRUCTION.
4. NO MECHANICAL TAMPERS SHALL BE USED DIRECTLY OVER PIPE TO INSURE PIPE IS NOT DAMAGED.
5. PROVIDE 8" OF COVER OVER PIPE IN PAVED AREAS AND 5" OF COVER OVER PIPE IN UNPAVED AREAS. REFER TO INSULATION DETAIL FOR AREAS WHERE PROPER COVER CAN NOT BE ACHIEVED.

DRAINAGE TRENCH DETAIL

NOT TO SCALE

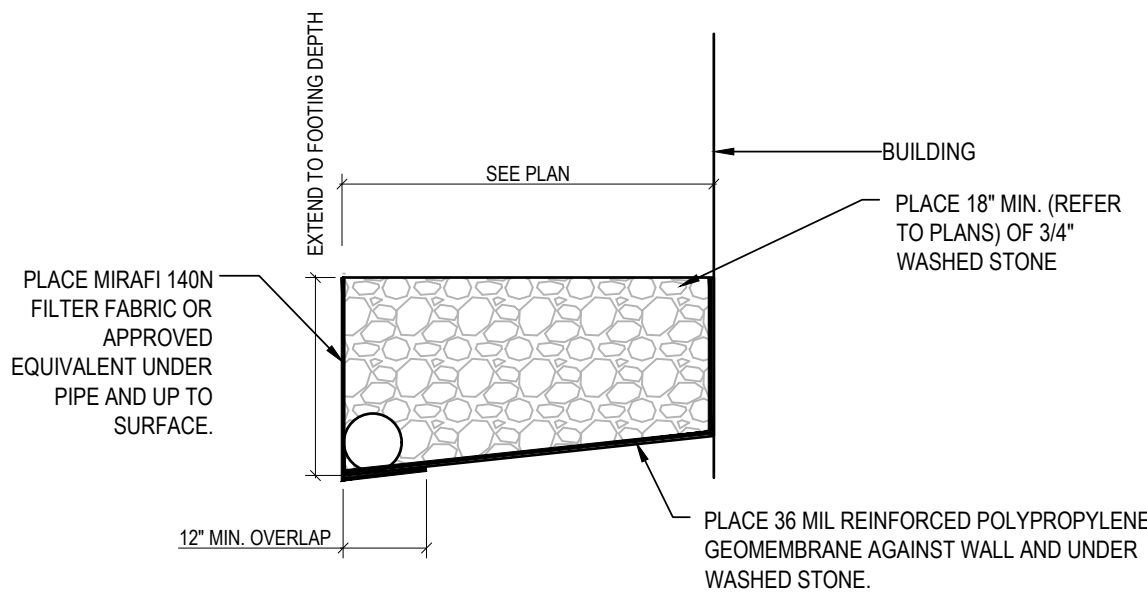
4



FLARED END SECTION W/STONE APRON

NOT TO SCALE

5



TYPICAL DRIP EDGE

NOT TO SCALE

6

SITE/EARTHWORK SPECIFICATIONS

1. PRIOR TO THE START OF WORK, A PRE-CONSTRUCTION MEETING WILL BE HELD WITH THE CONTRACTOR, AND PROJECT ENGINEER TO REVIEW PROCEDURES, IDENTIFY RESPONSIBILITIES. UNLESS STATED OTHERWISE, ALL MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH THE MOST RECENT VERSION OF THE VTRANS SPECIFICATIONS.
2. CLEARING AND GRUBBING - SITE TO BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, INCLUDING DRIVEWAYS, STONE WALLS, AND GRASS AREAS. THE DRIVEWAY SUB-GRADE MATERIAL SHALL EXTEND ONE FOOT BEYOND THE EDGE OF PAVING.
3. COMPACTION OF ALL MATERIALS SHALL BE PERFORMED USING VIBRATORY ROLLERS AND WATER IN LIFTS OF NO GREATER THAN TWELVE INCHES. COMPACTION SHALL BE PERFORMED UNTIL THE REQUIRED DENSITY IS ACHIEVED. DENSITY SHALL BE DETERMINED BY ASTM D2922 AND SHALL NOT BE LESS THAN THE REQUIRED AMOUNT AS DETERMINED IN ACCORDANCE WITH ASTM D1557.
4. COMPACTION TESTING SHALL BE PERFORMED FOR EVERY LAYER OF MATERIAL PLACED AND FOR EVERY 1000 SQUARE FEET OF AREA.
5. PAVEMENT SHALL MEET THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AS PUBLISHED BY VTRANS. PAVEMENT SHALL NOT BE INSTALLED WHEN THE OUTSIDE AIR TEMPERATURE IS BELOW 40 DEGREES FAHRENHEIT, NOR WHEN THE ROAD BASE TEMPERATURE IS BELOW 40 DEGREES FAHRENHEIT. PAVEMENT SHALL NOT FALL BELOW 185 DEGREES FAHRENHEIT PRIOR TO THE COMPLETION OF ROLLING. PAVEMENT SHALL NOT BE INSTALLED WHEN THE SUBGRADE IS FROZEN OR THE GRADES ARE INCORRECT.
7. ALL REMAINING DISTURBED AREAS SHALL BE FERTILIZED AND SEEDED IN ACCORDANCE WITH APPLICABLE STATE SPECIFICATIONS FOR EROSION CONTROL.
8. THE SEEDING OF 10% OR GREATER SLOPES SHALL REQUIRE THE USE OF EROSION CONTROL MATTING.
9. ALL EARTHWORK MATERIALS SHALL BE OBTAINED FROM APPROVED SOURCES. THEY SHALL CONSIST OF SATISFACTORILY GRADED, FREE DRAINING MATERIAL, REASONABLY FREE FROM LOAM, SILT, CLAY AND ORGANIC MATERIAL. EARTHWORK MATERIALS SHALL MEET THE REQUIREMENTS OF THE FOLLOWING TABLES:

A. SAND BLANKET/BEDDING: VTRANS 703.03A	SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
	2 INCHES	100
	1-1/2 INCHES	90 - 100
	1/2 INCH	70 - 100
	NO. 4	60 - 100
	NO. 100	0 - 20
	NO. 200	0 - 8

B. 3/4" CRUSHED STONE: VTRANS 704.02B	SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
	1 INCH	100
	3/4 INCHES	90 - 100
	3/8 INCH	20 - 55
	NO. 4	0 - 10
	NO. 8	0 - 5

C. 1-1/2" CRUSHED STONE: VTRANS 704.02C	SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
	2 INCH	100
	1-1/2 INCH	95 - 100
	3/4 INCH	35 - 70
	3/8 INCH	10 - 30
	NO. 4	0 - 5

D. COARSE CRUSHED GRAVEL: VTRANS 704.05A (COARSE)	SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
	4 INCHES	95 - 100
	NO. 4	25 - 50
	NO. 100	0 - 12
	NO. 200	0 - 6

E. FINE CRUSHED GRAVEL: VTRANS 704.05A (FINE)	SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
	2 INCHES	100
	1-1/2 INCHES	90 - 100
	NO. 4	30 - 60
	NO. 100	0 - 12
	NO. 200	0 - 6

F. GRANULAR BACKFILL: VTRANS 704.08A	SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
	3 INCHES	100
	NO. 4	45 - 75
	NO. 100	0 - 12
	NO. 200	0 - 6

- G. TYPE I STONE FOR STONE FILL THE LONGEST DIMENSION OF THE STONE SHALL VARY FROM 1 INCH TO 12 INCHES, AND AT LEAST 50 PERCENT OF THE VOLUME OF THE STONE IN PLACE SHALL HAVE A LEAST DIMENSION OF FOUR INCHES.
- H. TYPE II STONE FOR STONE FILL THE LONGEST DIMENSION OF THE STONE SHALL VARY FROM TWO INCHES TO 36 INCHES, AND AT LEAST 50 PERCENT OF THE VOLUME OF THE STONE IN PLACE SHALL HAVE A LEAST DIMENSION OF 12 INCHES.

- I. TOPSOIL SHALL MEET THE FOLLOWING REQUIREMENTS UNLESS OTHERWISE SPECIFICALLY STATED IN THE CONTRACT DOCUMENTS:

1. THE pH OF THE MATERIAL SHALL BE BETWEEN 5.5 AND 7.6.
2. THE ORGANIC CONTENT SHALL BE NOT LESS THAN 2% NOR MORE THAN 20%.
3. GRADATION:

SIEVE DESIGNATION	PERCENT BY WEIGHT PASSING SQUARE MESH SIEVES
2 INCHES	100
1 INCH	85 - 100
1/4 INCH	65 - 100
NO. 200	20 - 80

THE CONTRACTOR MAY AMEND NATURAL TOPSOIL WITH APPROVED MATERIALS AND BY APPROVED METHODS TO MEET THE ABOVE SPECIFICATIONS.

Stamp

Date

Description

No.

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Drainage Details

Brookside Apartments

Hartford, Windsor County, Vermont

EV Project #

24085

Drawn By:

AM

Checked By:

AM

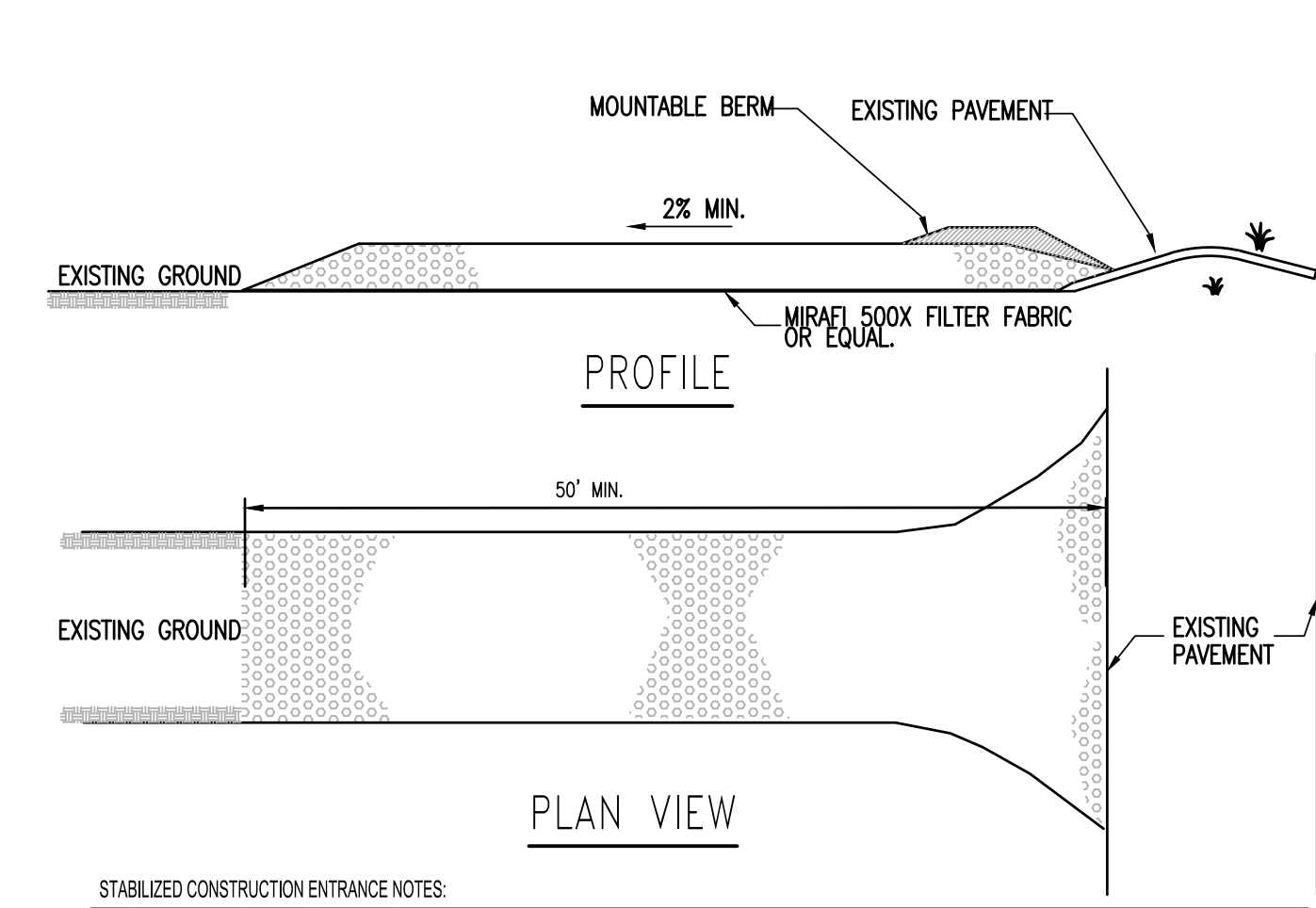
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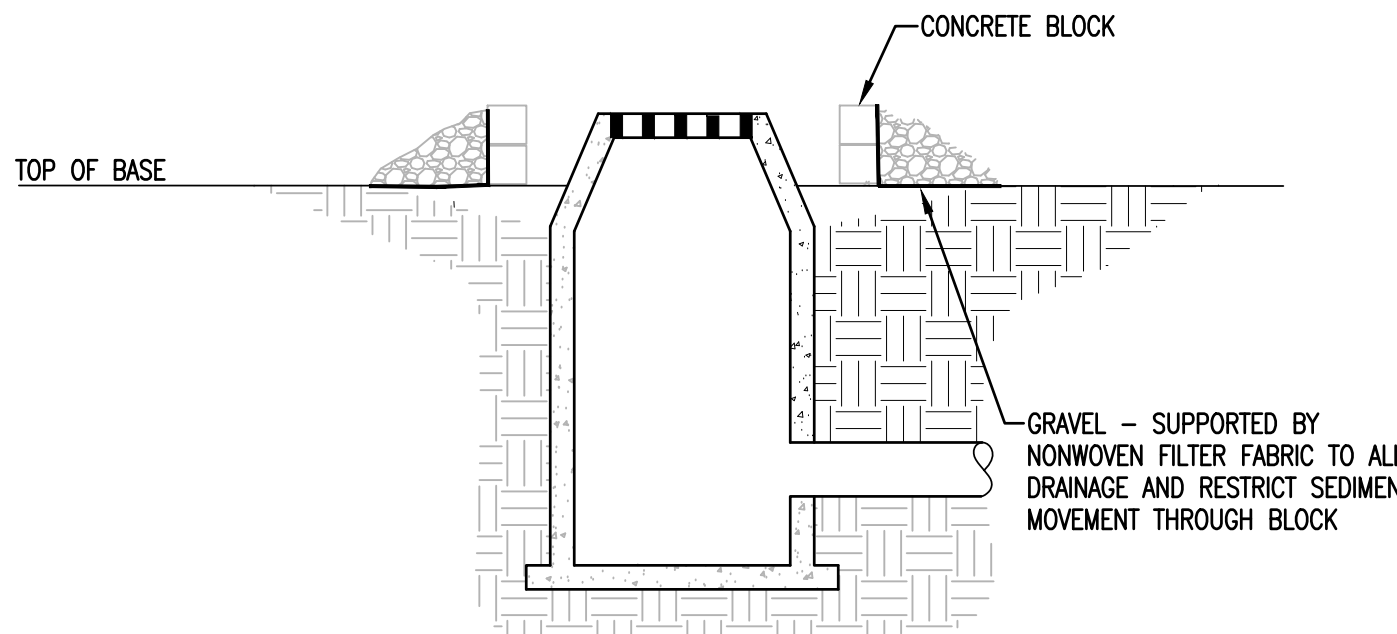


STABILIZED CONSTRUCTION ENTRANCE NOTES:

1. STONE SIZE: USE 1-1/2" CRUSHED STONE.
2. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCE SHALL BE PIPED ACROSS THE ENTRANCE.
3. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND. REPAIR AND/OR CLEANOUT ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC RIGHT-OF-WAYS MUST BE REMOVED IMMEDIATELY.
4. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.
5. WHEELS SHALL BE CLEANED TO REMOVE MUD PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.

CONSTRUCTION ENTRANCE DETAIL

SCALE: NONE

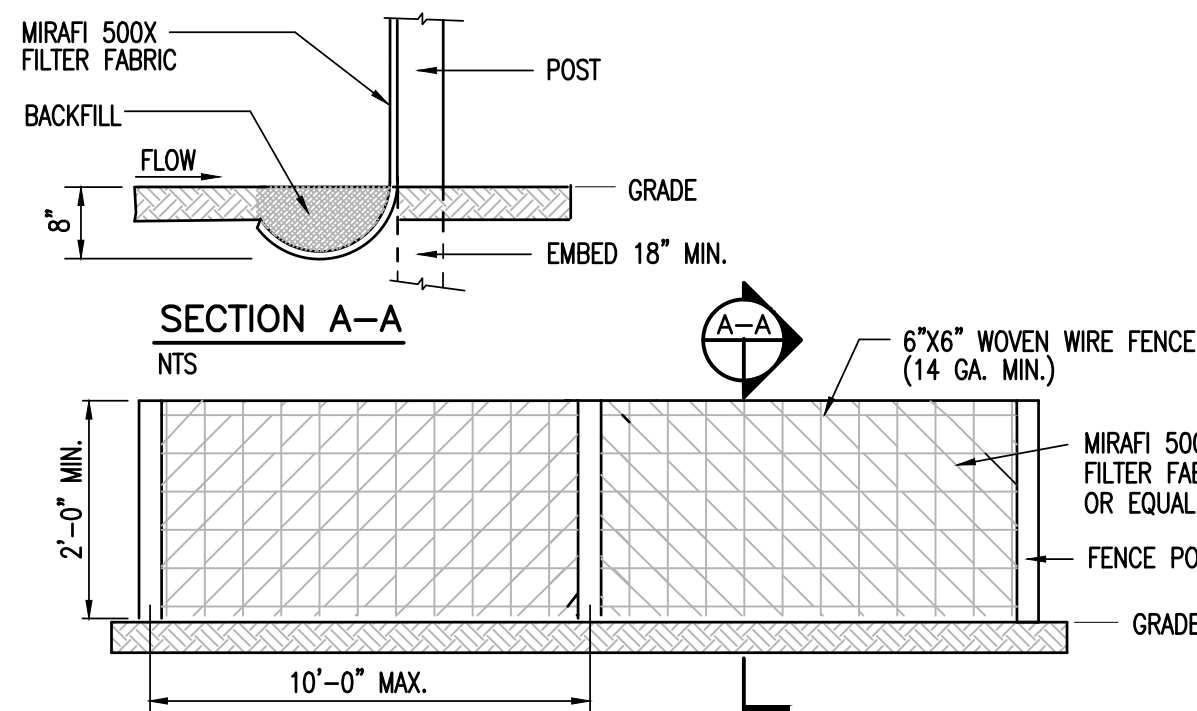


CONSTRUCTION SPECIFICATIONS

1. CLEAR THE AREA OF ALL DEBRIS THAT WILL HINDER FLOW.
2. GRADE APPROACH TO THE INLET UNIFORMLY AROUND THE BASIN.
3. UPON STABILIZATION OF CONTRIBUTING DRAINAGE AREA, REMOVE GRAVEL AND BLOCK AND STABILIZE SURROUNDING AREA PER PLAN.
4. GRAVEL BACKS, GRATE GUARDS, FILTERBOX OR SEDIGUARD INLET PROTECTION DEVICES MAY BE USED. SUBMIT PRODUCT INFORMATION TO ENGINEER FOR REVIEW PRIOR TO USE. INSTALL PRODUCTS IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS AND THE LATEST EDITION OF THE VT EPSC STANDARDS AND SPECIFICATIONS.

STONE AND BLOCK CATCH BASIN PROTECTION DETAIL

SCALE: NONE

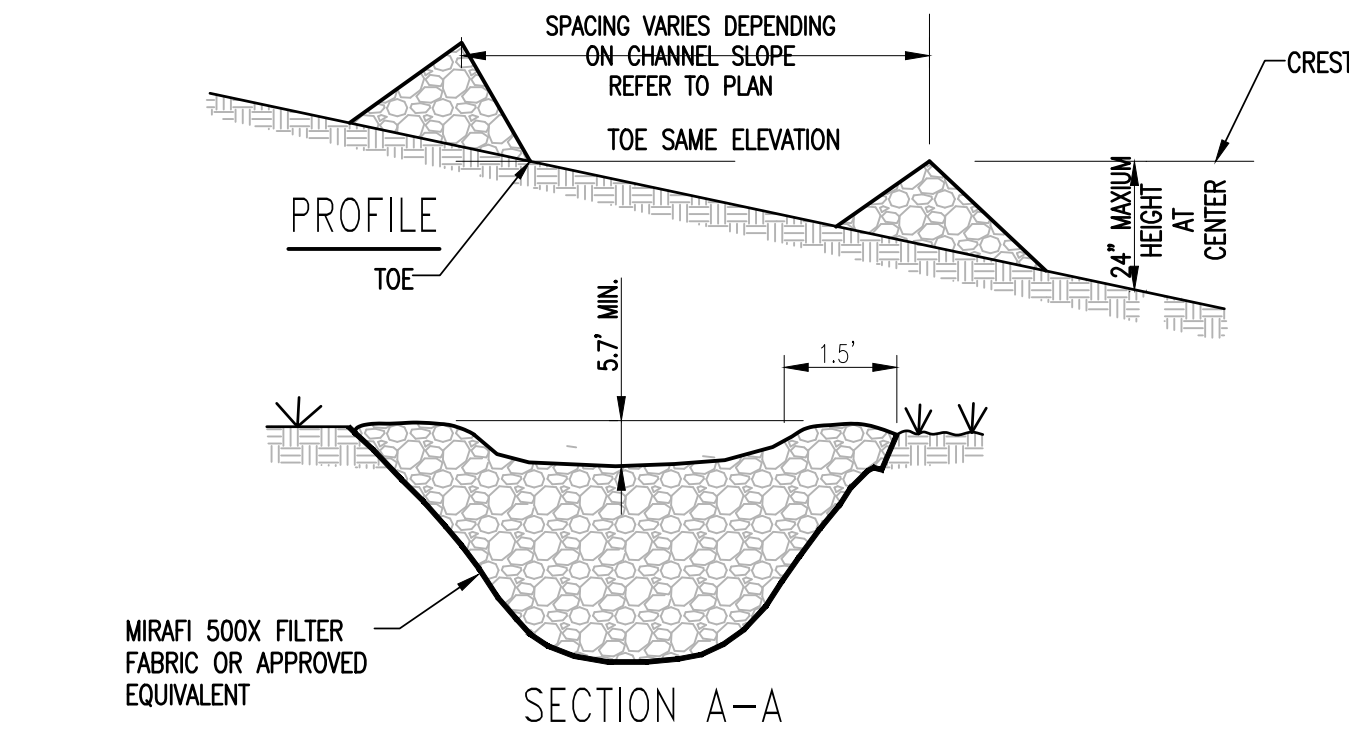


SILT FENCE NOTES:

1. SILT FENCE SHALL BE PRE-FABRICATED EROSION CONTROL FENCE BY MIRAFLO OR EQUAL, OR CONSTRUCTED IN PLACE AS SPECIFIED HEREIN.
2. CONSTRUCTED IN PLACE SILT FENCE:
 - A. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
 - B. FILTER FABRIC TO BE FASTENED SECURELY TO WOVEN WIRE FENCE TIES SPACED EVERY 24" AT TOP OF MID SECTION.
 - C. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED BY 6", FOLDED AND STAPLED.
3. INSPECTION SHALL BE FREQUENT (MINIMUM ONCE A WEEK AND AFTER EVERY RAINFALL). MAINTENANCE SHALL BE PERFORMED AS NEEDED, AND SEDIMENT REMOVED WHEN "BULGES" DEVELOP IN SILT FENCE.

SILT FENCE DETAIL

SCALE: NONE

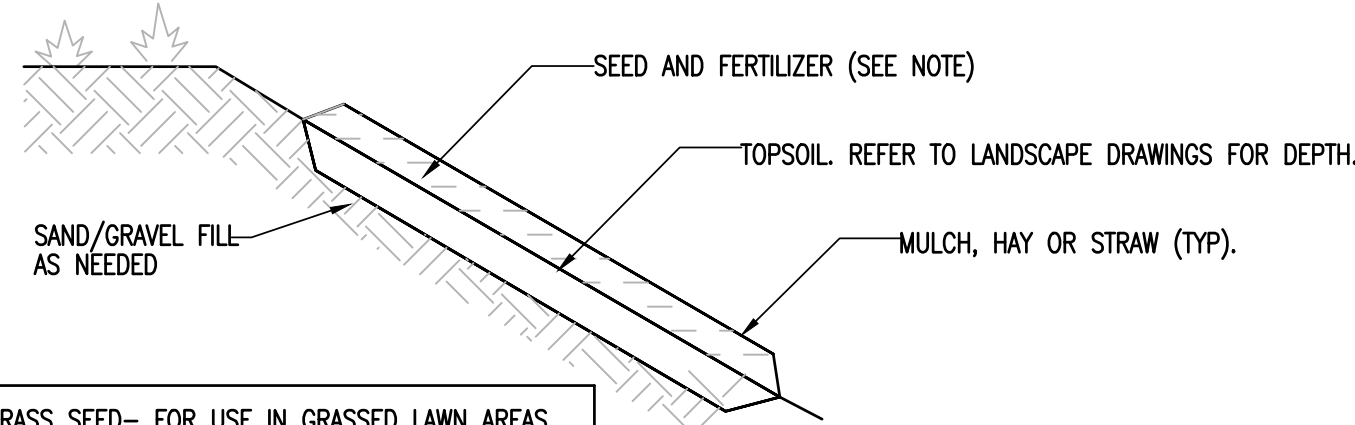


CHECK DAM NOTES

1. TYPE I STONE FILL WILL BE PLACED ON A FILTER FABRIC FOUNDATION AT THE GRADES AND LOCATIONS AS DESCRIBED HEREIN, AND TO MEET THE ABOVE DETAIL IN AREAS SHOWN ON PLANS
2. SET SPACING OF CHECK DAMS SUCH THAT THE ELEVATIONS OF THE CREST OF THE DOWNSTREAM DAM IS AT THE SAME ELEVATION OF THE TOE OF THE UPSTREAM DAM FOR A MINIMUM OF 100' DOWNSTREAM OF DISTURBED EARTH.
3. EXTEND THE STONE A MINIMUM OF 1.5 FEET BEYOND THE DITCH BANKS TO PREVENT CUTTING AROUND THE DAM.
4. PROTECT THE CHANNEL DOWNSTREAM OF THE LOWEST DAM FROM SCOUR AND EROSION WITH STONE OR LINER AS APPROPRIATE.
5. INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED WEEKLY (EVERY 7 DAYS) AND WITHIN 24 HOURS AFTER EACH RAIN.
6. REMOVE SEDIMENT COLLECTED BEHIND EACH DAM WHEN DEPTH OF SEDIMENT IS ONE HALF THE HEIGHT OF THE DAM.

STONE CHECK DAM DETAIL

SCALE: NONE



URBAN MIX GRASS SEED- FOR USE IN GRASSED LAWN AREAS AROUND BUILDING AND PARKING			
% BY WEIGHT	LBS. LIVE SEED BY	ACRE	TYPE OF SEED
44.44	20.0		CREeping RED FESCUE
44.44	20.0		KENTUCKY BLUEGRASS
11.11	5.0		RYEGRASS
100	45.0 # LIVE SEED/ ACRE		

CONSERVATION MIX GRASS SEED- FOR USE IN ALL OTHER AREAS		
% BY WEIGHT	LBS. LIVE SEED PER ACRE	TYPE OF SEED
50	15.0	SMOOTH BROMEGRASS
16.67	5.0	RYEGRASS
33.33	10.0	BIRDSFOOT TREFOIL
100	30.0 # LIVE SEED/ACRE	

FERTILIZER- 40 LBS. PER 1000S.F.
USE STANDARD 10-20-20 WHEN NO SOIL TEST DATA IS AVAILABLE
SPRING SEEDING
FALL SEEDING

LIME- 100 LBS. PER 1000S.F.
DOLOMITIC GROUND LIMESTONE
NOT LESS THAN 85% OF THE TOTAL CARBONATE

TOP SOIL
6" MINIMUM TOPSOIL

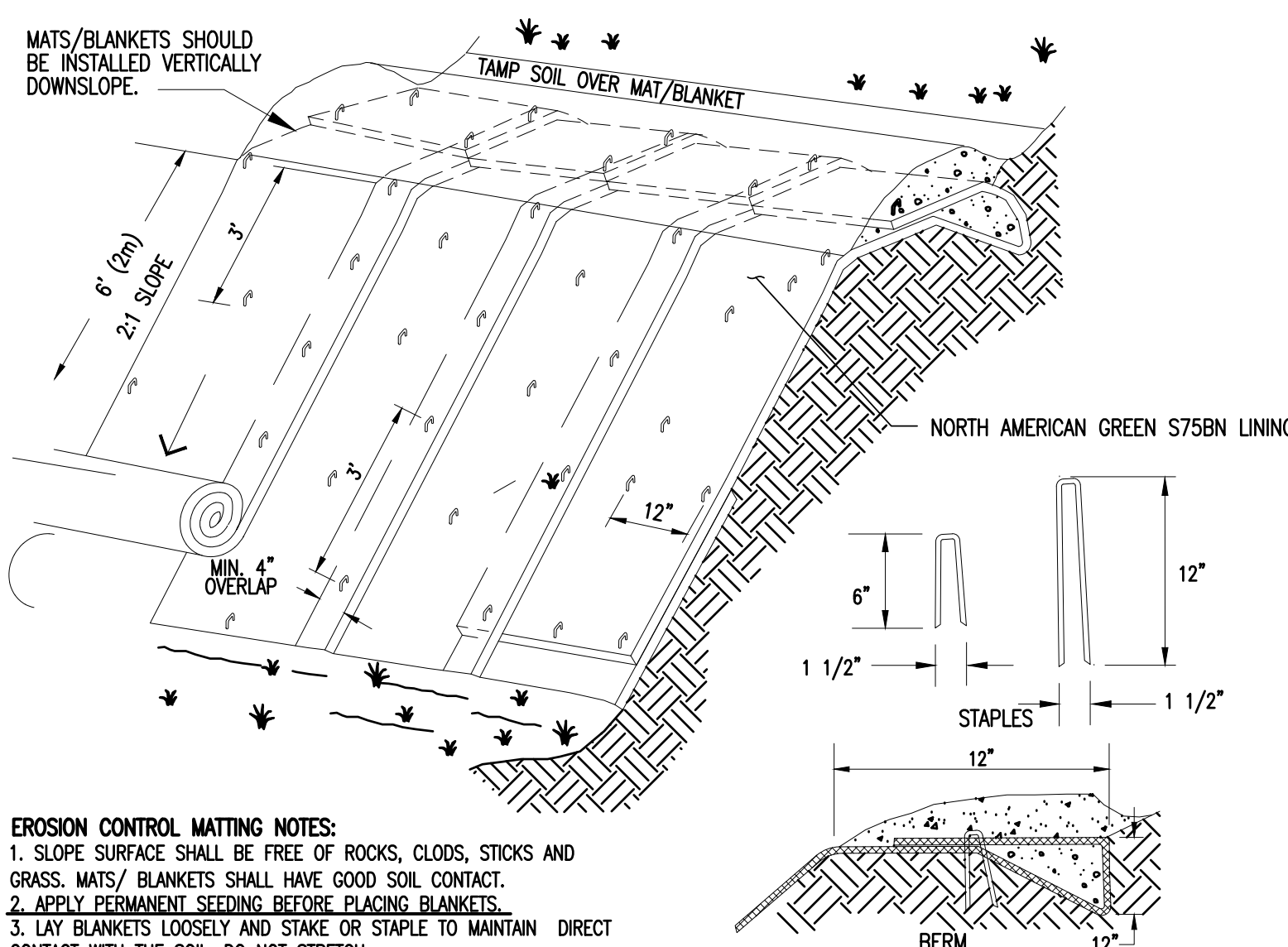
HAY OR STRAW MULCH- 90-100LBS. PER 1000S.F. APPLY BINDER OR NETTING AS NEEDED

NOTES FOR SEEDED AND MULCHED AREAS

1. ALL DISTURBED SURFACES SHALL BE TEMPORARILY OR PERMANENTLY SEEDED AND MULCHED WITHIN 7 DAYS OF DISTURBANCE.
2. SEEDING AND MULCHING OF DISTURBED AREAS SHALL TAKE PLACE WITHIN 48 HOURS OF FINAL GRADING.
3. MULCH: TYPICALLY HAY OR STRAW MAY BE UTILIZED AND SHALL BE APPLIED AT A RATE OF 90-100 LBS/1,000 SF. MULCH SHALL NOT BE PLACED ON SLOPES OF GREATER THAN 3:1. SEED IMPREGNATED EROSION CONTROL NETTING SHALL BE USED IN ITS PLACE.
4. SEED: SEEDING SHALL OCCUR AFTER APRIL 15 AND PRIOR TO SEPTEMBER 15TH IN ORDER TO ESTABLISH A STAND OF GRASS PRIOR TO GROUND FREEZING. SEED SHALL BE IN ACCORDANCE WITH SEED SPECIFICATION ON THIS SHEET.
5. COVER SEED WITH 1/2 INCH SOIL UNLESS A HYDROSEEDER IS USED.
6. MULCH ANCHORING: SHALL BE ACCOMPLISHED BY DEGRADABLE MULCH NETTING. USE WHEN SLOPES ARE GREATER THAN 10%.
7. TOPSOIL AND MULCHING NOT TO BE APPLIED IN AREAS OF TRAVEL WAYS.

SEEDED AND MULCHED AREAS DETAIL

SCALE: NONE

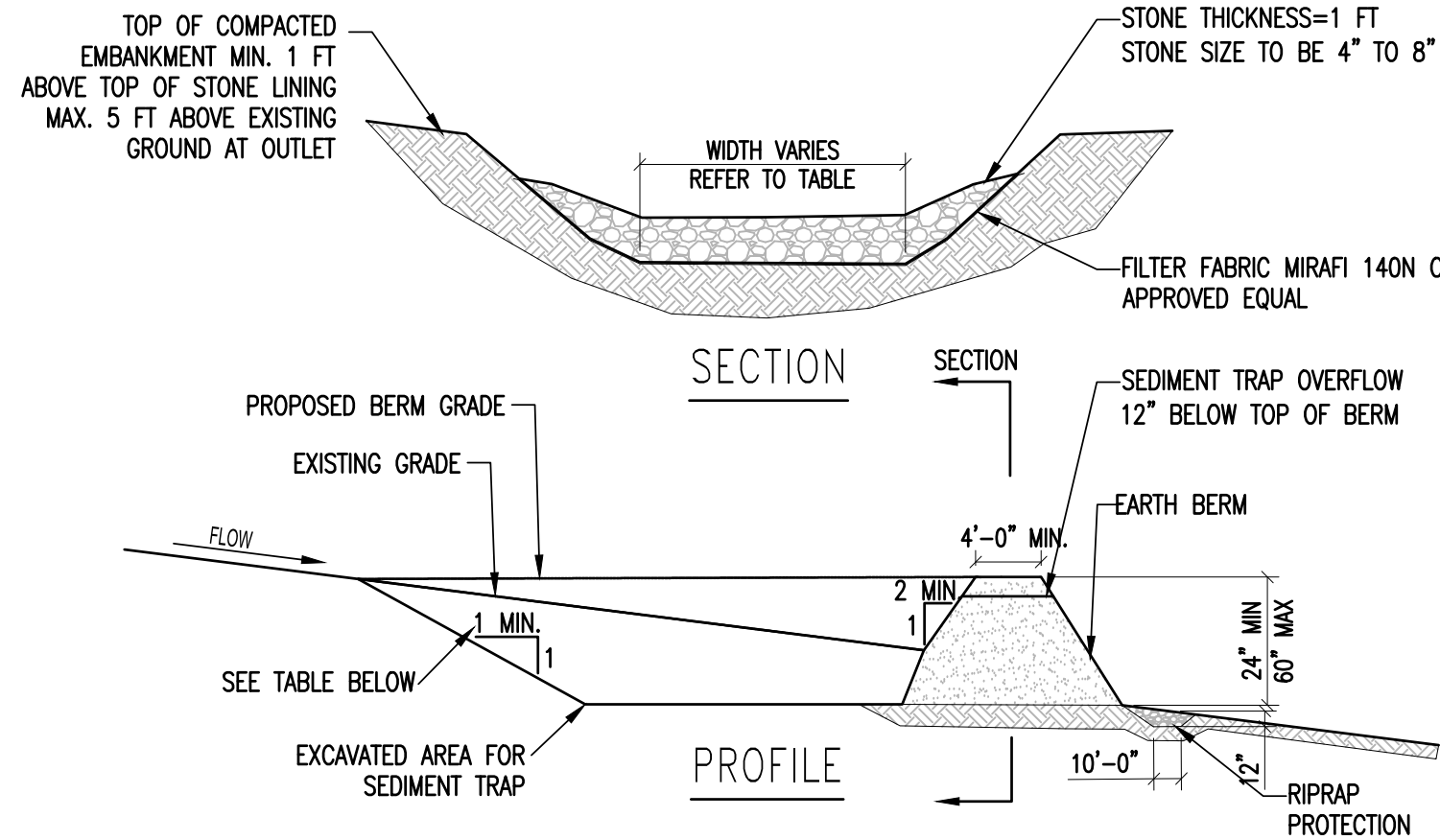


EROSION CONTROL MATTING NOTES:

1. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICKS AND GRASS. MATS/ BLANKETS SHALL HAVE GOOD SOIL CONTACT.
2. APPLY PERMANENT SEEDING BEFORE PLACING BLANKETS.
3. LAY BLANKETS LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH THE SOIL. DO NOT STRETCH.
4. MATTING SHALL BE NATURAL FIBER.

EROSION CONTROL MATTING

SCALE: NONE



SEDIMENT TRAP SCHEDULE AND NOTES

TRAP	DRAINAGE AREA IN ACRES	REQUIRED VOLUME OF SEDIMENT TRAP*	VOLUME PROVIDED	OVERFLOW WEIR ELEVATION	WEIR WIDTH	BOTTOM OF TRAP	TOP OF TRAP	SIDE SLOPE
TST-1	0.5	1800 CF	TBD	TBD	4'	TBD	TBD	2:1

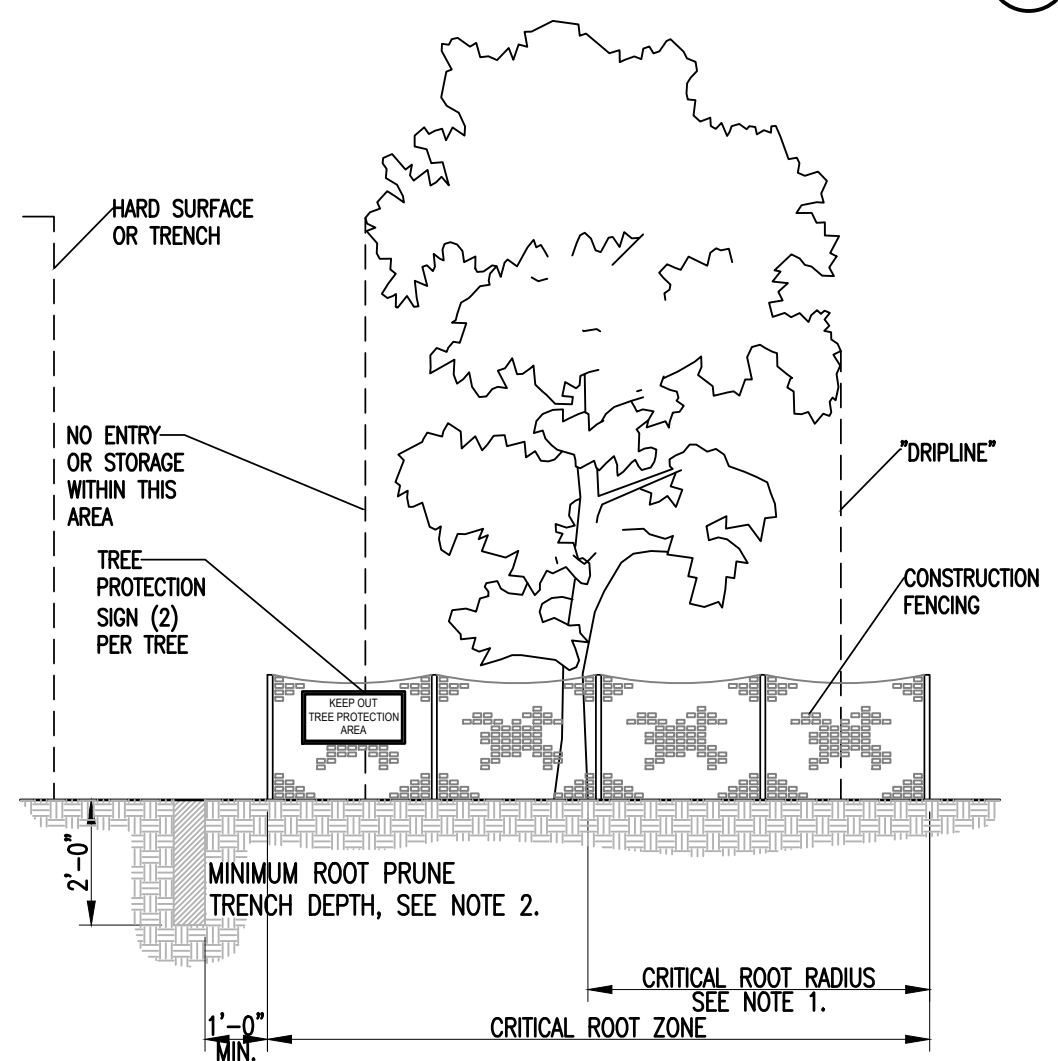
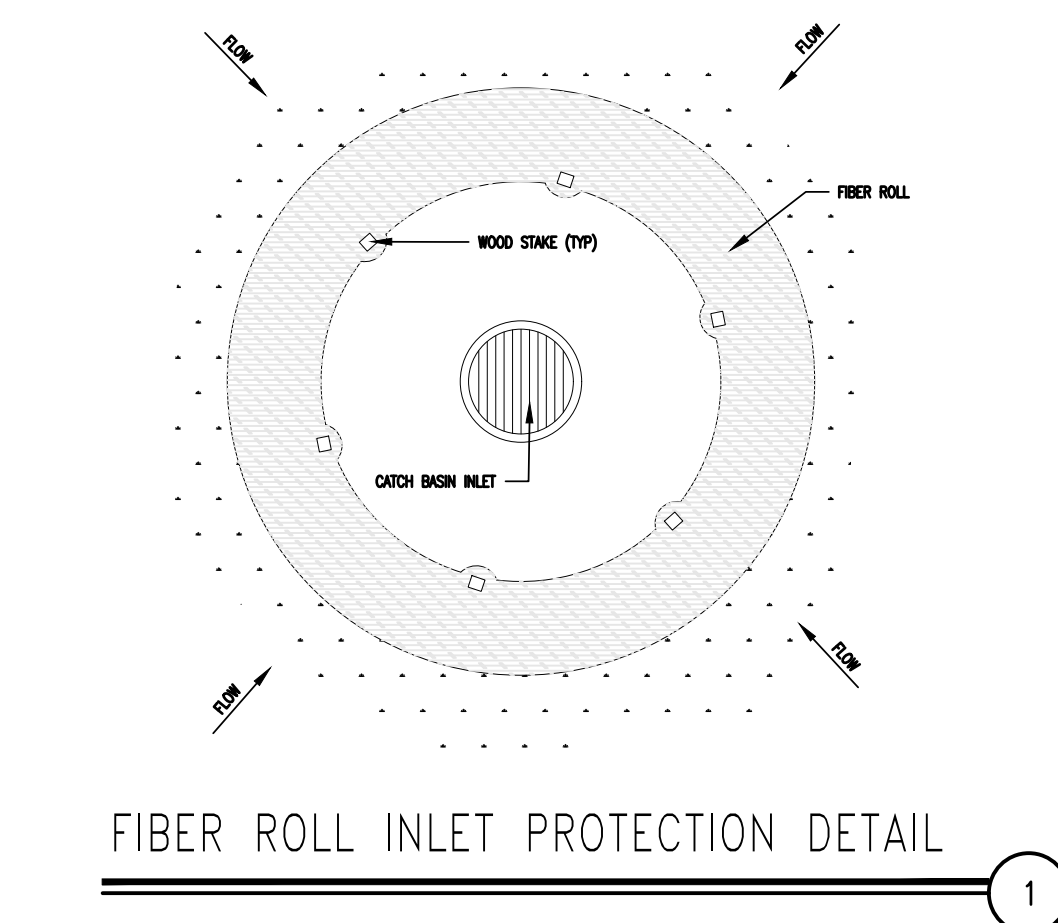
* SEE NOTE 5 BELOW

1. AREA UNDER EMBANKMENT SHALL BE CLEARED, GRUBBED, AND STRIPPED OF ANY VEGETATION AND ROOT MAT. THE POOL AREA SHALL BE CLEARED.
2. THE FILL MATERIAL FOR THE EMBANKMENT SHALL BE FREE OF ROOTS OR OTHER WOODY VEGETATION AS WELL AS OVERSIZED STONE, ROCKS, ORGANIC MATERIAL OR OTHER OBJECTIONABLE MATERIAL. THE EMBANKMENT SHALL BE COMPACTED BY TRAVERSING WITH EQUIPMENT WHILE IT IS BEING CONSTRUCTED. MAXIMUM HEIGHT OF EMBANKMENT SHALL BE FIVE (5) FEET, MEASURED AT CENTERLINE OF EMBANKMENT.
3. ANY EXCAVATED PORTION OF SEDIMENT TRAP SHALL HAVE 2:1 SLOPES OR FLATTER.
4. ELEVATION OF THE TOP OF ANY DIKE DIRECTING WATER INTO TRAP MUST EQUAL OR EXCEED THE HEIGHT OF EMBANKMENT.
5. THE VOLUME OF SEDIMENT STORAGE SHALL BE 3,600 CF PER ACRE OF CONTRIBUTORY DRAINAGE. MINIMUM VOLUMES AND OUTLET SIZES FOR EACH TRAP ARE LISTED IN THE SEDIMENT TRAP SCHEDULE SHOWN ABOVE.
6. FILTER CLOTH SHALL BE PLACED OVER THE BOTTOM AND SIDES OF THE OUTLET CHANNEL PRIOR TO PLACEMENT OF STONE. SECTIONS OF FABRIC MUST OVERLAP AT LEAST ONE (1) FOOT WITH SECTION NEAREST THE ENTRANCE PLACE ON TOP. FABRIC SHALL BE EMBEDDED AT LEAST SIX (6) INCHES INTO EXISTING GROUND AT ENTRANCE OUTLET CHANNEL.
7. STONE USED IN THE OUTLET CHANNEL SHALL BE FOUR (4) TO EIGHT (8) INCH RIP-RAP. TO PROVIDE A FILTERING EFFECT, A LAYER OF FILTER CLOTH SHALL BE EMBEDDED ONE (1) FOOT WITH SECTION NEAREST THE ENTRANCE PLACE ON TOP. FABRIC SHALL BE EMBEDDED AT LEAST SIX (6) INCHES INTO EXISTING GROUND AT ENTRANCE OUTLET CHANNEL.
8. SEDIMENT SHALL BE REMOVED AND THE TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED TO ONE-HALF OF THE DESIGN DEPTH OF THE TRAP. REMOVED SEDIMENT SHALL BE DEPOSITED IN A SUITABLE AREA AND STABILIZED.
9. THE STRUCTURE SHALL BE INSPECTED AFTER EACH RAIN AND REPAIRED AS NEEDED.
10. CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND SEDIMENT ARE CONTROLLED.
11. THE STRUCTURE SHALL BE REMOVED AND AREA BROUGHT TO FINAL GRADE AND THE AREA STABILIZED WHEN THE DRAINAGE AREA HAS BEEN PROPERLY STABILIZED.
12. DRAINAGE AREA FOR THIS PRACTICE IS LIMITED TO 15 ACRES OR LESS.
13. SEDIMENT TRAPS SHALL MEET THE VERMONT HANDBOOK FOR SOIL EROSION AND SEDIMENT CONTROL ON CONSTRUCTION SITES.

TEMPORARY SEDIMENT TRAP

NOT TO SCALE

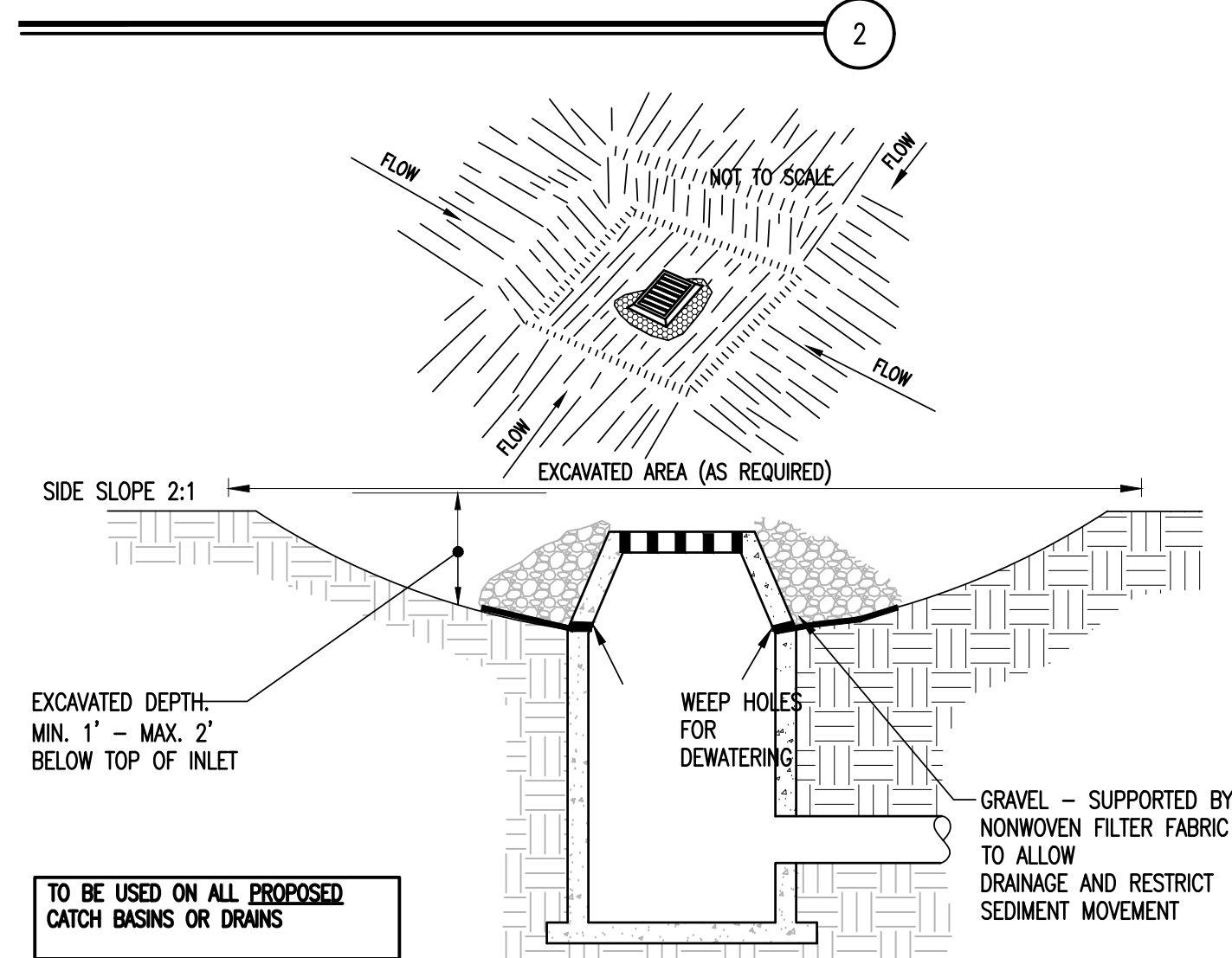
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ENGINEERING VENTURES PC	
208 Flynn Avenue, Suite 2A, Burlington, VT 05401 • 802-863-6225 85 Mechanic Street, Suite E2-3, Lebanon, NH 03766 • 603-442-3333 414 Union Street, Schenectady, NY 12305 • 518-630-9614 www.engineeringventures.com	
12 Tremont Street, LLC 77 Bank Street Lebanon, New Hampshire 603-727-9024	
Erosion Prevention and Sediment Control Details (1)	
Brookside Apartments Hartford, Windsor County, Vermont	
EV Project #	24086
Drawn By:	AM
Checked By:	AM
Scale:	NTS
Date:	7/8/2024
C3.2	



TREE PROTECTION NOTES:

1. TO CALCULATE THE CRITICAL ROOT RADIUS, ESTIMATE THE TREE'S HEIGHT AND MULTIPLY BY 40 PERCENT (0.40). THE RESULT IS THE APPROXIMATE DISTANCE FROM THE TREE TRUNK TO THE EDGE OF THE CONSTRUCTION FENCING.
2. FENCE SPAMEN TREES AND GROUPS OF TREES, WHERE ROOT LOSS WILL OCCUR, ROOT PRUNE ONE FOOT BEYOND THE CONSTRUCTION FENCING USING A VIBRATING KNIFE OR NARROW TRENCHER - ALWAYS WITH SHARP BLADES TO MAKE CLEAN CUTS. BACKFILL IMMEDIATELY AND COVER WITH 3 INCHES OF MULCH.
3. INSTALL SILL FENCE ON THE INTERIOR OF THE CONSTRUCTION FENCING TO KEEP SOIL FROM DISTURBED AREAS OUT OF THE ROOT ZONES OF TREES TO BE SAVED.
4. FERTILIZE, WATER, AERATE AND OTHERWISE ADJUST SOIL IN THE PROTECTED AREAS.
5. COORDINATE THE FINAL LOCATION OF THE CONSTRUCTION FENCING AND ROOT PRUNING WITH THE OWNER.

TREE PROTECTION DETAIL

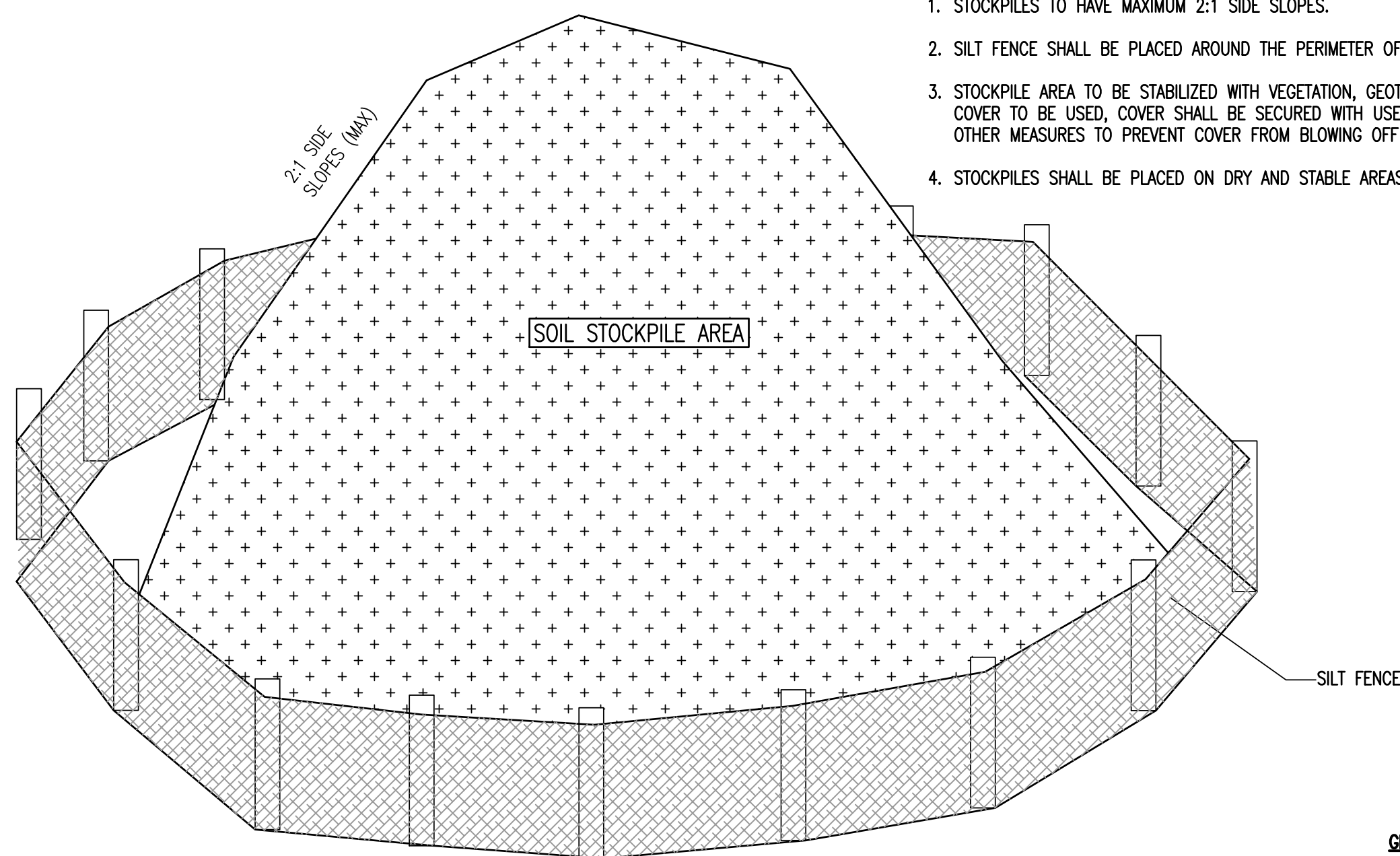


CONSTRUCTION SPECIFICATIONS

1. CLEAR THE AREA OF ALL DEBRIS THAT WILL HINDER EXCAVATION.
2. GRADE APPROACH TO THE INLET UNIFORMLY AROUND THE BASIN.
3. WEEP HOLES SHALL BE PROTECTED BY GRAVEL.
4. UPON STABILIZATION OF CONTRIBUTING DRAINAGE AREA, SEAL WEEP HOLES, FILL BASIN WITH STABLE SOIL TO FINAL GRADE, COMPACT IT PROPERLY AND STABILIZE WITH PERMANENT SEEDING.
5. GRAVEL BAGS, CRATE GUARDS, FILTREX OR SEDGEMUD INLET PROTECTION DEVICES MAY BE USED. SUBMIT PRODUCT INFORMATION TO ENGINEER FOR REVIEW PRIOR TO USE. INSTANT PRODUCTS IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS AND THE LATEST EDITION OF THE VT EPSC STANDARDS AND SPECIFICATIONS.

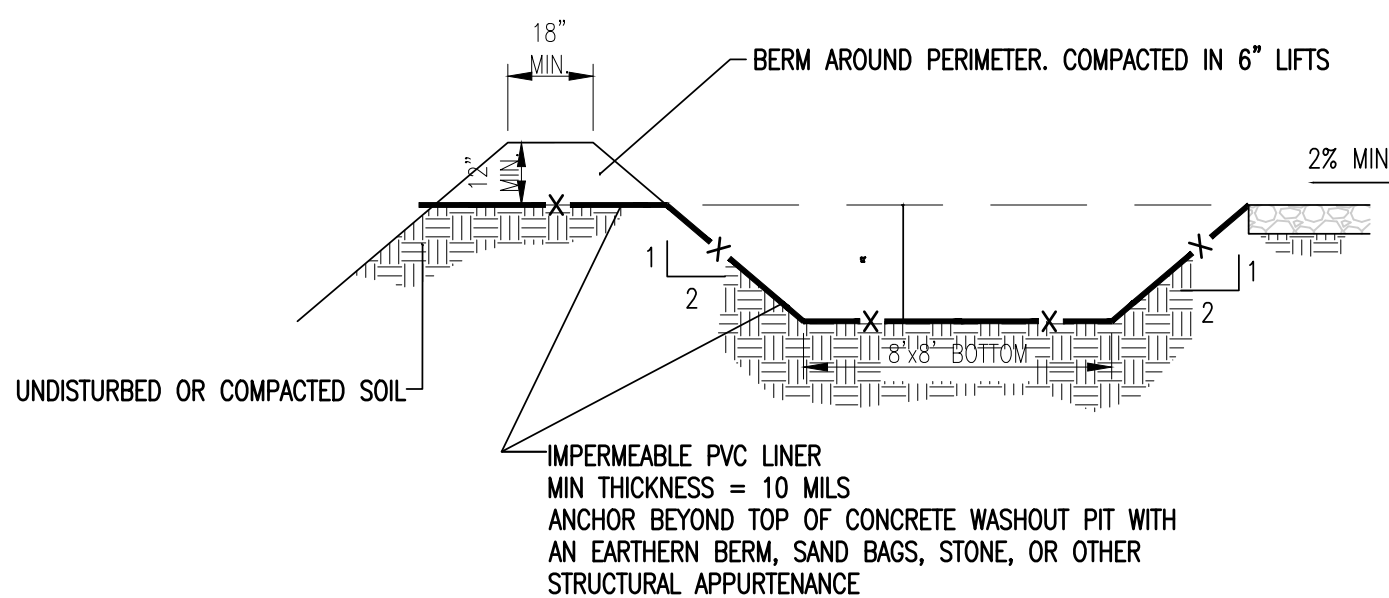
EXCAVATED STORM DRAIN INLET PROTECTION DETAIL

NOT TO SCALE



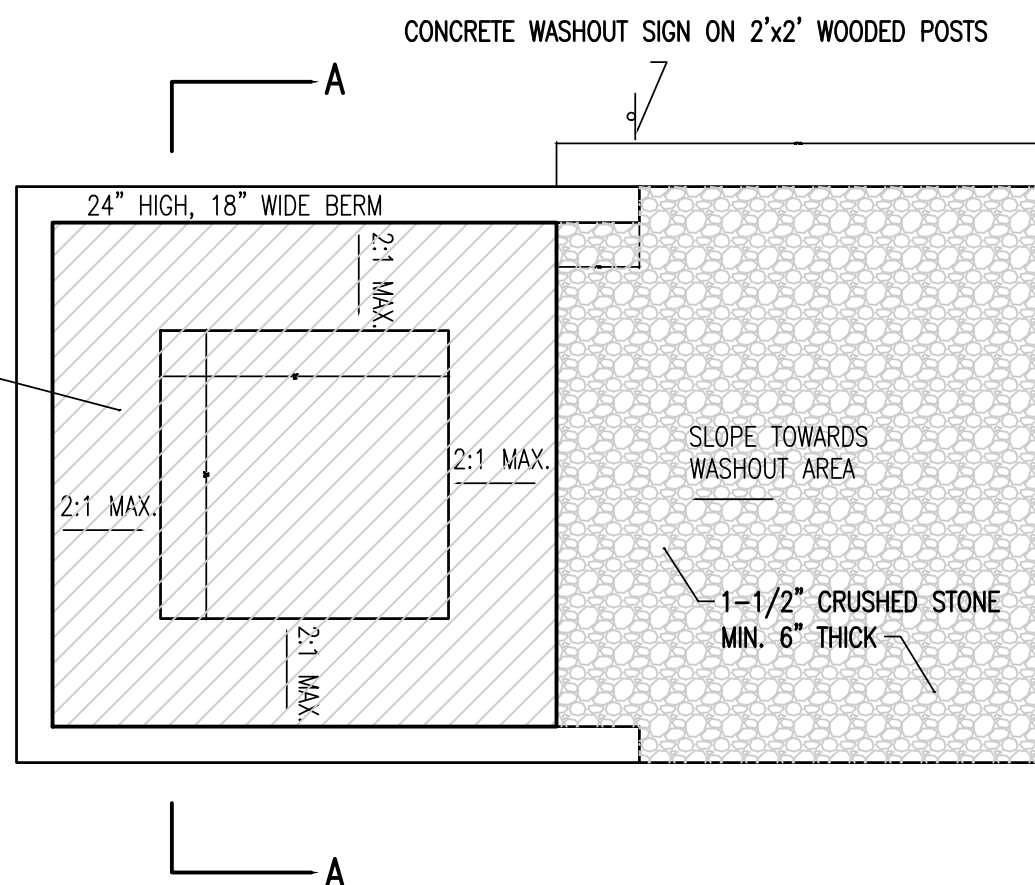
TEMPORARY SOIL STOCKPILE AREA DETAIL

SCALE: NONE



CONCRETE WASHOUT AREA NOTES

1. ALL CONCRETE WASHOUT FACILITIES SHALL BE INSPECTED DAILY. DAMAGED OR LEAKING FACILITIES SHALL BE DEACTIVATED AND REPAIRED/REPLACED IMMEDIATELY. EXCESS RAINWATER THAT HAS ACCUMULATED OVER HARDENED CONCRETE SHOULD BE PUMPED TO A STABILIZED AREA, SUCH AS A GRASS FILTER STRIP.
2. ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE CAPACITY OF THE STRUCTURE IS FILLED. ANY EXCESS WASH WATER SHALL BE PUMPED INTO A CONTAINMENT VESSEL AND PROPERLY DISPOSED OF OFF-SITE.
3. DISPOSE OF THE HARDENED MATERIAL OFF-SITE IN AN APPROPRIATE FACILITY. ON-SITE DISPOSAL MAY BE ALLOWED IF THIS HAS BEEN APPROVED AND ACCEPTED AS PART OF THE PROJECTS SWPPP. IN THAT CASE, THE MATERIAL SHALL BE RECYCLED AS SPECIFIED, OR BURIED AND COVERED WITH A MINIMUM OF 2 FEET OF CLEAN COMPACTED EARTHILL, THAT IS PERMANENTLY STABILIZED TO PREVENT EROSION.
4. ALL PLASTIC SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.
5. INSPECT THE PROJECT SITE FREQUENTLY TO ENSURE THAT NO CONCRETE DISCHARGES ARE TAKING PLACE IN NON-DESIGNATED AREAS.
6. IN LIEU OF CONCRETE WASHOUT AREA, A PRE-FABRICATED WASHOUT MAY BE USED. CONTRACTOR SHALL SUBMIT SHOP DRAWING OF SUCH PRE-FABRICATED WASHOUT AREA TO DESIGN ENGINEER FOR REVIEW AND APPROVAL.



CONCRETE TRUCK WASHOUT AREA

SCALE: NONE

STOCKPILE NOTES

1. STOCKPILES TO HAVE MAXIMUM 2:1 SIDE SLOPES.
2. SILT FENCE SHALL BE PLACED AROUND THE PERIMETER OF STOCKPILE AREA.
3. STOCKPILE AREA TO BE STABILIZED WITH VEGETATION, GEOTEXTILE, OR COVER. COVER TO BE USED, COVER SHALL BE SECURED WITH USE OF SAND BAGS OR OTHER MEASURES TO PREVENT COVER FROM BLOWING OFF STOCKPILE.
4. STOCKPILES SHALL BE PLACED ON DRY AND STABLE AREAS.

WINTER EROSION CONTROL NOTES

WINTER CONSTRUCTION PROCEDURES

1. DURING WINTER CONSTRUCTION, INSPECTIONS BY THE ON-SITE PLAN COORDINATOR SHALL OCCUR DAILY WHEN AREAS ARE UN-STABLE, AND WEEKLY PRIOR TO ANY FORECASTED RAIN, THAW OR SPRING MELT WHEN TEMPORARY STABILIZATION IS IN PLACE.
2. IN AREAS TO BE STABILIZED BY VEGETATION, ALL SEEDING MUST BE COMPLETED BY SEPTEMBER 15 TO ALLOW GROWTH TO OCCUR PRIOR TO THE GROUND FREEZING. STABILIZATION OF ALL OTHER DISTURBED AREAS SHALL BE COMPLETED BY OCTOBER 15.
3. ENLARGED ACCESS POINTS, STABILIZED TO PROVIDE FOR SNOW STOCKPILING SHALL BE INSTALLED.
4. LIMITS OF DISTURBANCE SHALL BE MOVED OR REPLACED TO REFLECT BOUNDARY OF WINTER WORK.
5. SNOW WILL NOT BE PILED WITHIN 25 FEET OF PERIMETER CONTROLS (SUCH AS SILT FENCE) TO ALLOW FOR CLEARING AND MAINTENANCE. SNOW IS TO BE REMOVED FROM ALL STRUCTURAL, EROSION PREVENTION AND SEDIMENTATION CONTROL MEASURES FOLLOWING EACH SIGNIFICANT SNOWFALL. NO SNOW STORAGE UP-GRADE OF DISTURBANCE. NO SNOW DISPOSAL IN SEDIMENT POND/S/BASINS. IF NECESSARY, SNOW/ICE MUST BE REMOVED PRIOR TO STABILIZATION OF DISTURBED AREAS. ACCESS POINTS SHALL BE ENLARGED AND STABILIZED TO ALLOW FOR SNOW STOCKPILING.
6. IN AREAS OF DISTURBANCE WITHIN 100 FT OF A RECEIVING WATER, SILT FENCE SHALL BE REINFORCED OR ELSE REPLACED WITH PERIMETER DIKES, SWALES OR OTHER PRACTICES RESISTANT TO THE FORCES OF SNOW LOADS.
7. DRAINAGE STRUCTURES SHALL BE KEPT OPEN AND FREE OF SNOW AND ICE DAMS.
8. ALL EROSION PREVENTION AND SEDIMENT CONTROL MEASURES ARE TO BE IN PLACE BY OCTOBER 15, OR IF NOT POSSIBLE, THEN PRIOR TO GROUND FREEZE.
9. MULCH IS TO BE APPLIED AT THE END OF EACH WORKDAY TO ALL EXPOSED AREAS THAT HAVE NOT YET REACHED FINAL GRADE AT TWICE THE RATE INDICATED IN THE SEEDING AND MULCHING DETAIL FOR THE REGULAR CONSTRUCTION SEASON. MULCH SHALL BE TRACKED IN OR STABILIZED WITH NETTING.
10. TO ENSURE COVER OF DISTURBED SOIL IN ADVANCE OF A MELT EVENT, AREAS OF DISTURBED SOIL MUST BE STABILIZED AT THE END OF EACH WORK DAY, WITH THE FOLLOWING EXCEPTIONS:
 - A) IF NO PRECIPITATION WITHIN 24 HOURS IS FORECAST AND WORK WILL RESUME IN THE SAME DISTURBED AREA WITHIN 24 HOURS, DAILY STABILIZATION IS NOT NECESSARY.
 - B) DISTURBED AREAS THAT COLLECT AND RETAIN RUNOFF, SUCH AS HOUSE FOUNDATIONS OR OPEN UTILITY TRENCHES.
11. SNOW AND ICE SHALL BE REMOVED TO LESS THAN 1" THICKNESS PRIOR TO STABILIZATION.
12. STONE STABILIZATION, 10 TO 20 FT WIDE IN AREAS SUCH AS THE PERIMETER OF BUILDINGS UNDER CONSTRUCTION WHERE CONSTRUCTION VEHICLE TRAFFIC IS ANTICIPATED SHALL BE INSTALLED.

GENERAL EROSION CONTROL NOTES

1. THE "ON-SITE EROSION CONTROL PLAN COORDINATOR" SHALL BE THE GENERAL CONTRACTOR, THIS INDIVIDUAL SHALL BE PRESENT ON-SITE FROM DAY-TO-DAY, AND SHALL BE RESPONSIBLE FOR ENSURING THAT THE EROSION CONTROL MEASURES REQUIRED BY THE EROSION CONTROL PLAN, DETAILS AND NOTES, ARE PROPERLY INSTALLED AND MAINTAINED. THE ONSITE EROSION CONTROL PLAN COORDINATOR SHALL KEEP A WRITTEN RECORD OF INSPECTIONS AND MAINTENANCE OF EROSION CONTROL FEATURES. A COPY OF THESE PLANS AND INSPECTION/MAINTENANCE RECORDS SHALL BE KEPT ONSITE AT ALL TIMES.
2. THE SMALLEST PRACTICAL AREA OF LAND SHALL BE DISTURBED AT ANY ONE TIME DURING DEVELOPMENT. WHEN LAND IS DISTURBED, THE DISTURBANCE SHALL BE KEPT TO THE SHORTEST PRACTICAL DURATION AS APPROVED BY THE OWNER'S REP.
3. DUST SHALL BE CONTROLLED WITH WATER DISTRIBUTED BY A TRUCK MOUNTED SPRAY BAR. CALCIUM CHLORIDE (AASHTO M 144) OR SODIUM CHLORIDE (AASHTO M 143) SHALL BE USED AS DIRECTED BY THE OWNER'S REP.
4. ALL EROSION AND STORMWATER CONTROL SYSTEMS SHALL BE INSPECTED ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS OF THE OCCURRENCE OF A STORM EVENT OF 0.5" OR GREATER. NEEDED REPAIRS SHALL BE MADE IMMEDIATELY. SEDIMENT DEPOSITS SHALL BE REMOVED AS THEY ACCUMULATE AND PLACED IN AREAS WHERE FURTHER EROSION IS UNLIKELY.
5. A REPORT, STAMPED BY A QUALIFIED ENGINEER OR A CPESC SPECIALIST, SHALL BE WRITTEN WITHIN 24 HOURS OF EACH INSPECTION THAT:
 - A) DESCRIBES THE PROGRESS OF THE PROJECT, INCLUDING WHETHER ALL CONDITIONS OF THE PERMIT ARE BEING MET AND, IF NOT, WHICH REQUIREMENTS ARE NOT BEING MET;
 - B) IF ANY REQUIREMENTS ARE NOT BEING MET, AN EXPLANATION OF THE CORRECTIVE ACTION(S) THAT WILL BE OR ARE BEING TAKEN TO BRING THE PROJECT INTO COMPLIANCE WITH APPLICABLE REQUIREMENTS AND THE DEADLINE BY WHICH SUCH ACTIONS WILL BE COMPLETED; AND
 - C) INCLUDES PHOTOGRAPHS OF THE SITE THAT ARE REPRESENTATIVE OF THE PROJECT; AND
6. RETAIN A COPY OF THE REPORT PREPARED PURSUANT TO (5), ABOVE, ON-SITE FOR REVIEW DURING SITE INSPECTIONS BY FEDERAL, STATE, AND LOCAL OFFICIALS.
7. ALL PERIMETER CONTROLS SHALL BE INSTALLED PRIOR TO EARTH MOVING OPERATIONS.
8. DETENTION BASINS AND SWALES SHALL BE INSTALLED PRIOR TO ROUGH GRADING THE SITE.
9. ALL DITCHES AND SWALES SHALL BE STABILIZED PRIOR TO DIRECTING RUNOFF TO THEM.
10. ALL ROADWAYS AND PARKING LOTS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
11. ALL CUT AND FILL SLOPES SHALL BE SEEDED/LOAMED WITHIN 72 HOURS OF ACHIEVING FINISHED GRADE.
12. AN AREA SHALL BE CONSIDERED STABLE IF ONE OF THE FOLLOWING HAS OCCURRED:
 - BASE COURSE GRADES HAVE BEEN INSTALLED IN AREAS TO BE PAVED;
 - A MINIMUM OF 85% VEGETATED GROWTH HAS BEEN ESTABLISHED;
 - MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIPRAP HAS BEEN INSTALLED; OR
 - EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.
13. LOT DISTURBANCE OTHER THAN THAT SHOWN ON THE APPROVED PLANS SHALL NOT COMMENCE UNTIL AFTER THE ROADWAY HAS THE BASE COURSE TO DESIGN ELEVATION AND THE ASSOCIATED DRAINAGE IS COMPLETE AND STABLE.
14. THE APPLICANT SHALL NOTIFY IN WRITING DES WETLANDS BUREAU AND THE LEBANON CONSERVATION COMMISSION OF THEIR INTENTION TO COMMENCE CONSTRUCTION NO LESS THAN FIVE (5) BUSINESS DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
15. APPROPRIATE SILTATION/EROSION/TURBIDITY CONTROLS SHALL BE IN PLACE PRIOR TO CONSTRUCTION, SHALL BE MAINTAINED DURING CONSTRUCTION, AND REMAIN IN PLACE UNTIL THE AREA IS STABILIZED. SILT FENCE(S) MUST BE REMOVED ONCE THE AREA IS STABILIZED.
16. DISCHARGE FROM DEWATERING OF WORK AREAS SHALL BE TO SEDIMENT BASINS THAT ARE:
 - A) LOCATED IN UPLANDS;
 - B) LINED WITH HAY BALES OR OTHER ACCEPTABLE SEDIMENT TRAPPING LINERS;
 - C) SET BACK AS FAR AS POSSIBLE FROM WETLANDS AND SURFACE WATERS, IN ALL CASES WITH A MINIMUM OF 20 FEET OF UNDISTURBED VEGETATED BUFFER.
17. WITHIN THREE DAYS OF FINAL GRADING, ALL EXPOSED SOIL SURFACE SHALL BE STABILIZED BY SEEDING AND MULCHING DURING THE GROWING SEASON, OR IF NOT WITHIN THE GROWING SEASON, BY MULCHING WITH TACK OR NETTING AND PINNING ON SLOPES STEEPER THAN 3:1.
18. WHERE CONSTRUCTION ACTIVITIES HAVE BEEN TEMPORARILY SUSPENDED WITHIN THE GROWING SEASON, ALL EXPOSED SOIL AREAS SHALL BE STABILIZED WITHIN 14 DAYS BY SEEDING AND MULCHING.
19. WHERE CONSTRUCTION ACTIVITIES HAVE BEEN TEMPORARILY SUSPENDED OUTSIDE THE GROWING SEASON, ALL EXPOSED AREAS SHALL BE STABILIZED WITHIN 14 DAYS BY MULCHING AND TACK. SLOPES STEEPER THAN 3:1 SHALL BE STABILIZED BY MATTING AND PINNING.
20. PROPER HEADWALLS SHALL BE CONSTRUCTED WITHIN SEVEN DAYS OF CULVERT INSTALLATION.

EROSION CONTROL CONSTRUCTION SEQUENCE:

1. THE "ON-SITE EROSION CONTROL PLAN COORDINATOR" SHALL BE THE GENERAL CONTRACTOR, THIS INDIVIDUAL SHALL BE PRESENT ON SITE FROM DAY TO DAY, AND SHALL BE RESPONSIBLE FOR ENSURING THAT THE EROSION CONTROL MEASURES REQUIRED BY THE EROSION CONTROL PLAN, DETAILS AND NOTES, ARE PROPERLY INSTALLED AND MAINTAINED. THE ONSITE EROSION CONTROL PLAN COORDINATOR SHALL KEEP A WRITTEN RECORD OF INSPECTIONS AND MAINTENANCE OF EROSION CONTROL FEATURES. A COPY OF THESE PLANS AND INSPECTION/MAINTENANCE RECORDS SHALL BE KEPT ONSITE AT ALL TIMES.
2. HOLD PRE-CONSTRUCTION CONFERENCE AT LEAST ONE WEEK PRIOR TO STARTING CONSTRUCTION, WITH THE CONTRACTOR, OWNER, AND ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING ALL EROSION CONTROL MEASURES DELINEATED ON THE PLANS AND ANY ADDITIONAL MEASURES THAT ARE NECESSARY TO MINIMIZE EROSION. THE CONTRACTOR SHALL HAVE EROSION CONTROL MATERIALS AND EQUIPMENT AVAILABLE ON-SITE AT ALL TIMES.
3. IF POSSIBLE, ALL TEMPORARY EROSION CONTROL MEASURES REQUIRED FOR WORK PROPOSED DURING THE WINTER (BETWEEN OCTOBER 15 AND MAY 1), SHALL BE INSTALLED PRIOR TO OCTOBER 1.
4. PRIOR TO ANY OTHER WORK, SILT FENCES SHALL BE INSTALLED GENERALLY 10 FEET FROM THE BASE OF THE FILL SLOPES, OR AS SHOWN ON THE EROSION CONTROL PLAN. THESE SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE PROJECT SITE HAS BEEN STABILIZED. SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE WHEN IT BECOMES 6 INCHES DEEP AT THE FENCE. THE SILT FENCE WILL BE REPAIRED AS NECESSARY TO MAINTAIN A PROPER SEDIMENT BARRIER.
5. INSTALL TEMPORARY GRAVEL CONSTRUCTION ENTRANCE/EXIT.
6. INSTALL INLET PROTECTION ON EXISTING AND NEW CATCH BASINS.
7. ROUGH GRADE AND STOCKPILE TOPSOIL SURROUNDED BY SILT FENCE.
8. STABILIZE EXPOSED SLOPES AND SOILS AS SOON AS GRADED, AND MAINTAIN UNTIL ADEQUATELY VEGETATED.
9. COMPLETE FINAL GRADING OF SITE, PLACE TOPSOIL AND PERMANENTLY VEGETATE, LANDSCAPE, AND MULCH.
10. AFTER THE SITE IS APPROVED BY THE ENGINEER AS ADEQUATELY STABILIZED, REMOVE ALL TEMPORARY MEASURES AND INSTALL PERMANENT VEGETATION ON THE DISTURBED AREAS.

TEMPORARY/CONSTRUCTION EROSION CONTROL MEASURES

1. SILT FENCES SHALL BE INSTALLED GENERALLY 10 FEET FROM THE BASE OF THE FILL SLOPES, OR AS SHOWN ON THE PLANS. THESE SHALL REMAIN IN PLACE UNTIL THE PROJECT SITE HAS BEEN STABILIZED.
2. SEDIMENT SHALL BE REMOVED FROM BEHIND THE SILT FENCE WHEN IT BECOMES 6 INCHES DEEP AT THE FENCE. THE SILT FENCE WILL BE REPAIRED AS NECESSARY TO MAINTAIN A PROPER SEDIMENT BARRIER.
3. INSTANT INSOLE CHECK DAMS IN GRASS- COVERED SLOPES 50 FEET ON CENTER TO PREVENT SILT FROM ENTERING THE DRAINAGE SYSTEM UNTIL DRAINAGE SYSTEM DURING CONSTRUCTION. SEDIMENT SHALL BE REMOVED FROM BEHIND THE DAM WHEN IT BECOMES 6 INCHES DEEP.
4. DROP INLET PROTECTION SHALL BE PROVIDED AROUND ALL EXISTING AND PROPOSED CATCH BASINS. PROTECTION SHALL REMAIN UNTIL ALL DISTURBED AREAS ARE STABILIZED. SEDIMENT SHALL BE REMOVED FROM DROP INLET PROTECTION WHEN THE STORAGE CAPACITY HAS BEEN APPROXIMATELY 50% FILLED. GRAVEL WILL BE CLEANED OR REPLACED WHEN IT NO LONGER DRAINS PROPERLY.
5. EXCAVATED MATERIAL FROM EARTH EXCAVATION AND DITCH DIGGING SHALL BE DISPOSED OF OFFSITE OR USED FOR PROJECT FILL MATERIAL IF DETERMINED SUITABLE BY THE OWNER'S REPRESENTATIVE.
6. STOCKPILES OF MATERIAL (TIMBER, ROCK, BORROW, ETC.) SHALL BE SILT FENCE CONSTRUCTED AROUND THE PERIMETER. THE STOCKPILED MATERIAL SHALL BE COVERED AND MULCHED AS SOON AS POSSIBLE TO PREVENT SOIL EROSION AND TO PREVENT WIND OFF SITE. LOCATE STOCKPILES ON THE UPHILL SIDE OF DISTURBED AREAS, IF POSSIBLE. DURING WINDY CONDITIONS, STOCKPILED MATERIAL SHALL BE COVERED OR WINDPROOF APPROPRIATELY TO PREVENT WIND EROSION.

PERMANENT EROSION CONTROL MEASURES:

1. GRASS LINED SWALES SHALL BE TOP SOILED, SODDED AND FERTILIZED AREAS WHICH EXHIBIT SIGNS OF EROSION SHALL BE RE-SODDED IMMEDIATELY AND MAINTAINED UNTIL SOD HAS PERMANENT HOLD AND IS HEALTHY.
2. WHEN CONSTRUCTION IS COMPLETED IN AN AREA, IT SHALL BE IMMEDIATELY TOP SOILED, SEEDDED, FERTILIZED AND MULCHED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTINUED MAINTENANCE OF ALL DISTURBED AREAS, INCLUDING WATCHING, UNTIL THE AREA IS INSPECTED AND ACCEPTED BY THE OWNER OR ENGINEER.
4. AFTER THE SITE IS STABILIZED, REMOVE ALL TEMPORARY MEASURES AND INSTALL PERMANENT VEGETATION ON THE DISTURBED AREAS.
5. RE-SEEDING SHALL BE DONE UNTIL ALL AREAS ARE COMPLETELY COVERED WITH A MATURE STRAND OF GRASS. AN AREA SHALL BE CONSIDERED COVERED WHEN THE ENTIRE SURFACE CONTAINS A VERDURIOUS STAND OF GRASS. AREAS THAT, IN THE OPINION OF THE ENGINEER, ARE PREDOMINATELY WEEDS SHALL BE PLOWED UP, FINE GRADED, FERTILIZED AND RE-SEEDING IN THE MANNER SPECIFIED PREVIOUSLY, EXERCISING CAUTION NOT TO CAUSE DAMAGE TO NEW OR EXISTING PLANT MATERIAL.

[illegible]

**ENGINEERING
VENTURES^{PC}**

Suite 2A, Burlington, VT 05401 • 802-863-6225
, Suite E2-3, Lebanon, NH 03766 • 603-442-9333
West, Schenectady, NY 12305 • 518-630-9614
www.engineeringventures.com

12 Tremont Street, LLC
77 Bank Street
Lebanon, New Hampshire
603-727-9024

Erosion Prevention and Sediment Control Details (2)

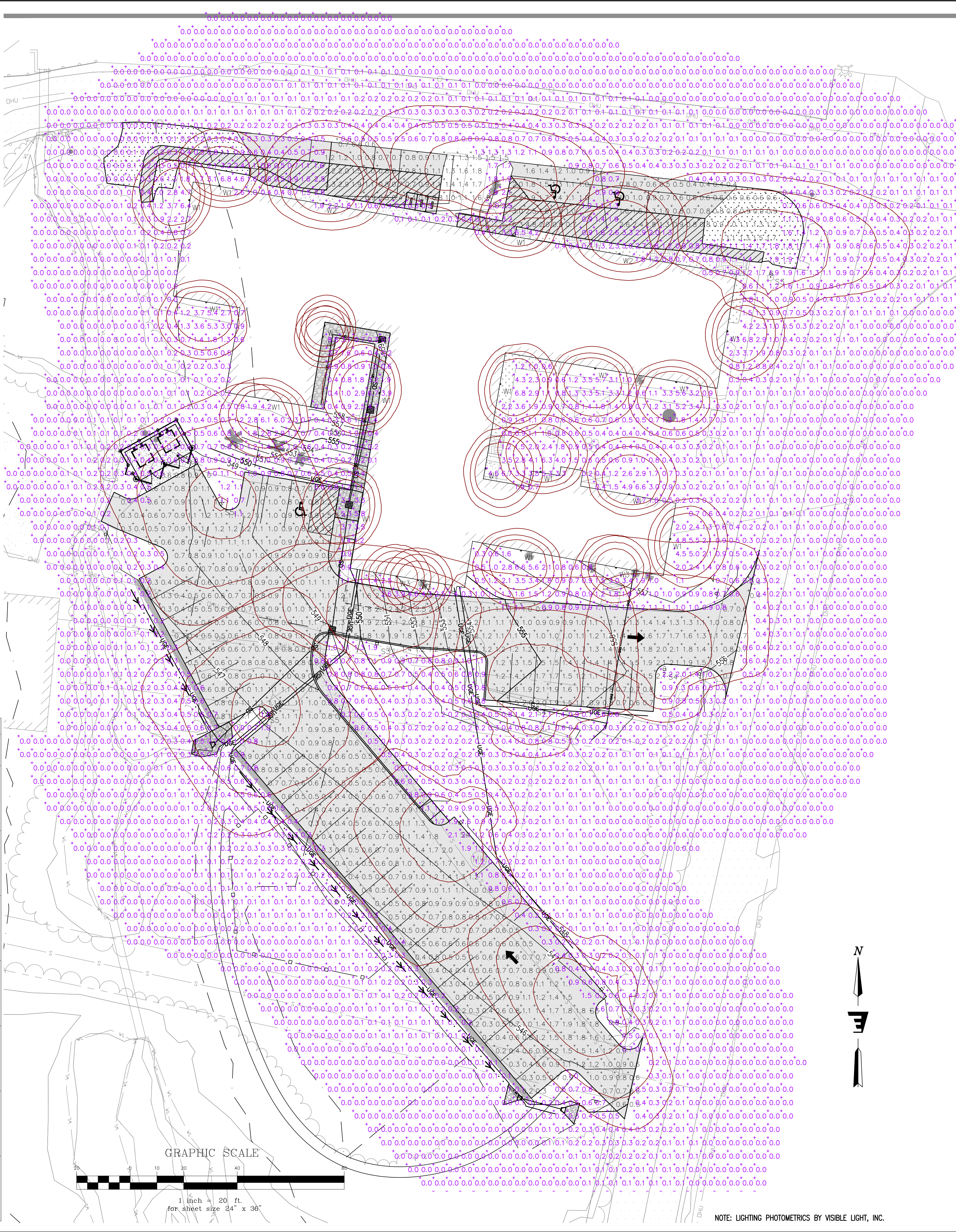
Brookside Apartments
Hartford, Windsor County, Vermont

EV Project #	24085
Drawn By:	AM
Checked By:	AM
Scale:	NTS
Date:	7/8/2024

C3.3

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Large Parking Lot	+	0.9 fc	3.4 fc	0.2 fc	17.0:1	4.5:1
Outside of Parking Lots	+	0.4 fc	8.8 fc	0.0 fc	N/A	N/A
Small Parking Lot	+	1.2 fc	6.3 fc	0.4 fc	15.8:1	3.0:1

Schedule											
Symbol	Label	QTY	Manufacturer	Catalog Number	Description	Lamp	Filename	Lumens per Lamp	LLF	Wattage	Distribution
	S2	1	Lithonia Lighting	DSX0 LED P1 30K 80CRI T2M MVOLT HS SPA DDBXD with SSS 12 4C DM19AS DDBXD	D-Series Size 0 Area Luminaire P1 Performance Package 3000K CCT 80 CRI Type 2 Medium Houseside Shield; mounted at 14ft (12ft pole on 2ft base)	LED	DSX0_LED_P1_30K_80CRI_T2M_HS.ies	3580	0.9	33.21	TYPE III, MEDIUM, BUG RATING: B1 – UO – G2
	S3	3	Lithonia Lighting	DSX0 LED P1 30K 80CRI T3M MVOLT HS SPA DDBXD with SSS 12 4C DM19AS DDBXD	D-Series Size 0 Area Luminaire P1 Performance Package 3000K CCT 80 CRI Type 3 Medium; mounted at 14ft (12ft pole on 2ft base)	LED	DSX0_LED_P1_30K_80CRI_T3M.ies	4173	0.9	33.21	TYPE IV, MEDIUM, BUG RATING: B1 – UO – G2
	S4	3	Lithonia Lighting	DSX0 LED P1 30K 80CRI T3M MVOLT SPA DDBXD with SSS 12 4C DM19AS DDBXD	D-Series Size 0 Area Luminaire P1 Performance Package 3000K CCT 80 CRI Forward Throw; mounted at 14ft (12ft pole on 2ft base)	LED	DSX0_LED_P1_30K_80CRI_T3M.ies	4265	0.9	33.21	TYPE IV, SHORT, BUG RATING: B1 – UO – G2
	S5	2	Lithonia Lighting	DSX0 LED P1 30K 80CRI T5M MVOLT SPA DDBXD with SSS 12 4C DM19AS DDBXD	D-Series Size 0 Area Luminaire P1 Performance Package 3000K CCT 80 CRI Type 5 Medium; mounted at 14ft (12ft pole on 2ft base)	LED	DSX0_LED_P1_30K_80CRI_T5M.ies	4358	0.9	33.21	TYPE VS, BUG RATING: B3 – UO – G1
	W1	16	Lithonia Lighting	ARC1 LED P1 30K MVOLT DDBXD	ARC1 LED WITH P1 – PERFORMANCE PACKAGE, 3000K; mounted at 8ft	LED	ARC1_LED_P1_30K.ies	1376	0.9	10.8751	TYPE III, VERY SHORT, BUG RATING: B1 – UO – G0
	W2	2	Lithonia Lighting	ARC1 LED P2 30K MVOLT DDBXD	ARC1 LED WITH P2 – PERFORMANCE PACKAGE, 3000K; mounted at 14ft	LED	ARC1_LED_P2_30K.ies	2035	0.9	16.7843	TYPE III, VERY SHORT, BUG RATING: B1 – UO – G1
	W3	5	Brownlee Lighting Inc	7317 BZ B06 WHA 30K	Shore Sconce; mounted at 7ft	LED	7317-B12-CLA-35K.ies	1607	0.4	6	



NOTE: LIGHTING PHOTOMETRICS BY VISIBLE LIGHT, INC.

Stamp	
Date	
Description	
No.	
ENGINEERING VENTURES PC	
208 Flynn Avenue, Suite 2A, Burlington, VT 05401 • 802-863-6225 885 Mechanic Street, Suite E2-3, Lebanon, NH 03766 • 603-442-9333 414 Union Street, Schenectady, NY 12305 • 518-530-3614 www.engineeringventures.com	
12 Tremont Street, LLC 77 Bank Street Lebanon, New Hampshire 603-727-9024	
Site Photometrics Plan	
Sheet Title:	Project Title:
EV Project #	24086
Drawn By:	AM
Checked By:	AM
Scale:	1" = 20'
Date:	7/8/2024
PH1	

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Hartford, Windsor County, Vermont

Project #	2423
Checked By:	PS
Drawn By:	PS
Scale:	1"=20'
Date:	07-08-2024

L1.0

EXISTING LANDSCAPING FEATURES TO REMAIN:



EXISTING GAZEBO AND FLAGPOLE (TO REMAIN)



BUILDING FOUNDATION LANDSCAPING ADDED (SEE L2.0)



EXISTING PERENNIAL PLANTINGS (TO REMAIN)



EXISTING PERIMETER TREES AND PLANTINGS (TO REMAIN)



OVERGROWN JUNIPER PLANTINGS (TO BE REMOVED)

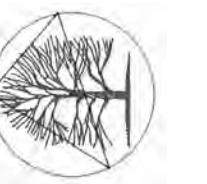


ENTRY PARKING AND LANDSCAPING RENOVATED (SEE L2.0)

[illegible]

ParkArchitecture.com

3 School House Lane, Suite #1 Etna NH 03750
Tel: 603-643-3400 Mobile: 617-905-0467
Email: parkarchitecture@gmail.com

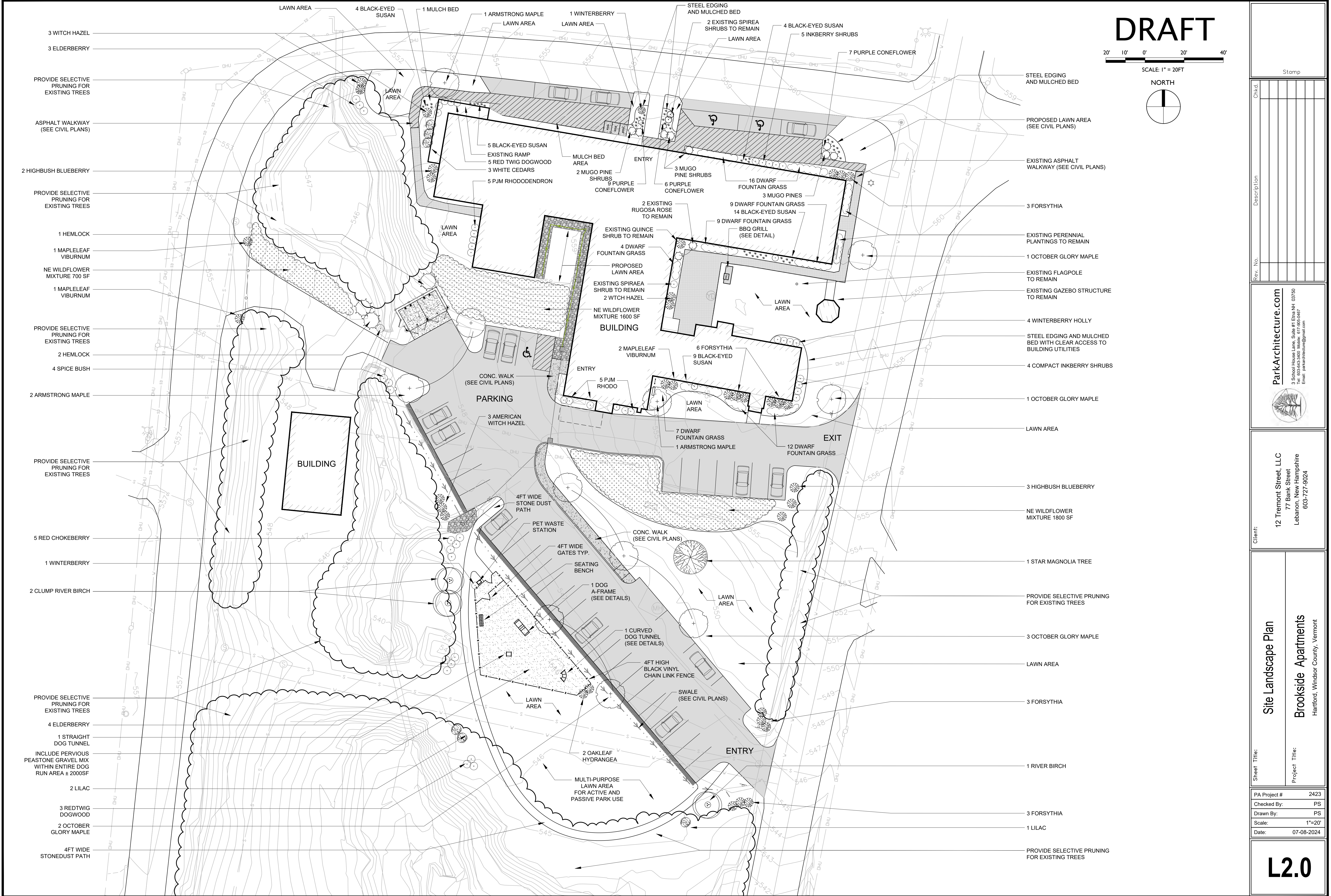


Client: 12 Tremont Street, LLC
77 Bank Street
Lebanon, New Hampshire
603-727-9024

Existing Conditions	Brookside Apartments Hartford, Windsor County, Vermont
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PA Project #	2423
Checked By:	PS
Drawn By:	PS
Scale:	1"=20'
Date:	07-08-2024

L1.1



L2.0
(color version)

- PROVIDE SELECTIVE PRUNING FOR EXISTING TREES

PROPOSED PLANT MATERIAL IMAGES:



RED CHOKEBERRY



ELDERBERRY



ELDERBERRY



WITCHHAZEL



PURPLE LILAC



M. VIBURNUM



HIGHBUSH BLUEBERRY



SPICEBUSH



BLACK-EYED SUSAN



OAKLEAF HYDRANGEA



INKBERRY SHRUB



PJM RHODODENDRON



WINTERBERRY



WINTERBERRY



RED TWIG DOGWOOD



RED TWIG DOGWOOD



FORSYTHIA



MUGHO PINE



DWARF FOUNTAIN GRASS

LANDSCAPE NOTES

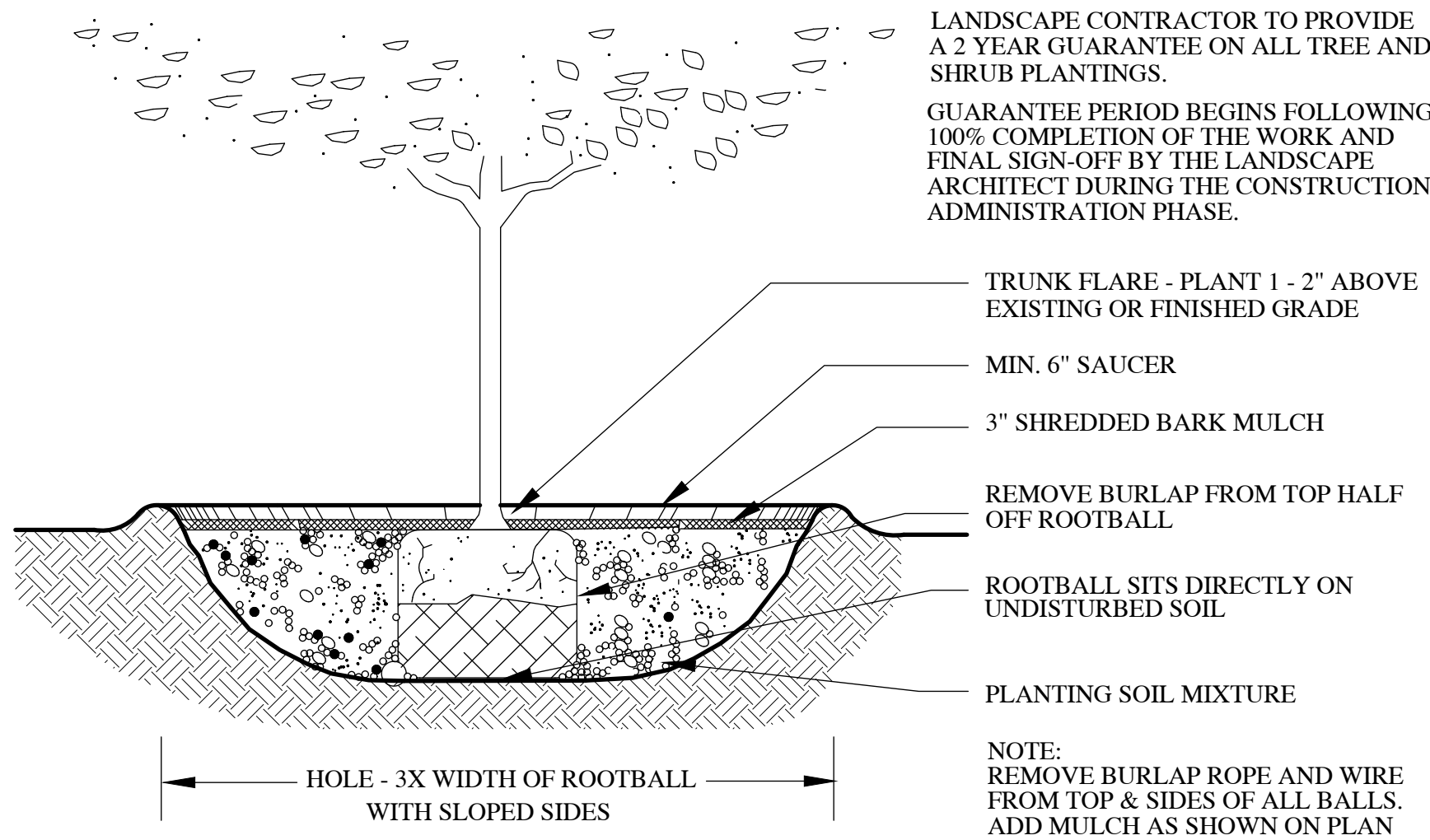
1. THE NUMBER OF EACH INDIVIDUAL PLANT TYPE AND SIZE IS PROVIDED IN THE PLANT LIST FOR CONTRACTORS CONVENIENCE ONLY. IF A DISCREPANCY EXISTS BETWEEN THE NUMBER OF PLANTS ON THE LIST AND THE NUMBER SHOWN ON THE DRAWING, THE GREATER NUMBER SHALL APPLY.
2. THE CONTRACTOR SHALL SUPPLY PLANT MATERIAL IN QUANTITIES SUFFICIENT TO COMPLETE THE PLANTING AS SHOWN.
3. ALL PLANT MATERIAL SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT OR THE OWNER'S REPRESENTATIVE PRIOR TO ARRIVAL ON SITE.
4. ALL PLANT BEDS WILL BE TOP-DRESSED WITH 3" SHREDDED BARK MULCH.
5. THE CONTRACTOR SHALL LOCATE AND MARK ALL UTILITIES PRIOR TO PLANTING. ANY CONFLICTS BETWEEN PLANTING AND UTILITIES SHALL BE IMMEDIATELY REPORTED TO THE OWNER SO THAT ALTERNATIVE PLANTING LOCATIONS CAN BE DETERMINED.
7. NO SUBSTITUTION OF PLANT MATERIALS WILL BE ALLOWED WITHOUT THE PRIOR WRITTEN APPROVAL OF THE OWNER OR LANDSCAPE ARCHITECT.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL PLANTS AGAINST DAMAGE FROM ON-GOING CONSTRUCTION. PROTECTION SHALL BEGIN AT THE TIME THE PLANT IS INSTALLED AND CONTINUE UNTIL FORMAL ACCEPTANCE OF ALL PLANTINGS.
9. ALL OTHER DISTURBED AREAS NOT OTHERWISE INDICATED SHALL BE LOAMED AND SEEDED.
10. CONTRACTOR SHALL PRESERVE EXISTING TREES AND EXISTING PLANTINGS WHERE INDICATED.
11. CONTRACTOR SHALL PROVIDE A 2-YEAR GUARANTEE ON ALL INSTALLED PLANT MATERIAL.
12. ALL PROPOSED TREES SHALL BE PLACED A MINIMUM OF 10FT FROM EXISTING AND PROPOSED UTILITIES.
13. ALL TREES SPECIFIED WILL BE OF GOOD QUALITY AND IN COMPLIANCE WITH THE MOST RECENT EDITION OF ANSI Z60.1 AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY AMERICANHORT.
14. SPECIAL ATTENTION SHALL BE PAID TO THE SITE PREPARATION AND SOILS FOR ALL PLANTING AREAS TO ENSURE THE HEALTH AND VIGOROUS GROWTH OF PLANTINGS (ESPECIALLY TREES AND SHRUBS). CONTRACTOR SHALL PROVIDE ADEQUATE SOIL VOLUME AS DEPICTED IN THE TREE AND SHRUB PLANTING DETAILS HEREIN.
15. SEE CIVIL PLANS FOR LOCATION OF SNOW STORAGE AREAS

LANDSCAPE PLANTING SCHEDULE

QTY	BOTANICAL NAME	COMMON NAME	SIZE	RIPARIAN VARIETY
DECIDUOUS TREES				
7	ACER RUBRUM OCTOBER GLORY	OCTOBER GLORY RED MAPLE	2.5" - 3" CALIPER	X
4	ACER X FREEMANII 'ARMSTRONG'	COLUMNAR RED MAPLE	2.5" - 3" CALIPER	X
3	BETULA NIGRA	CLUMP RIVER BIRCH	CLUMP 10'-12' HT.	X
1	MAGNOLIA STELLATA	STAR MAGNOLIA	60"-72" HT. B&B	
EVERGREEN TREES				
3	THUJA CANADENSIS	CANADIAN HEMLOCK	8'-10' HT. B&B	X
3	THUJA OCCIDENTALIS	NORTHERN WHITE CEDAR	8'-10' HT. B&B	X
SHRUBS				
8	HAMAMELIS VIRGINIANA	AMERICAN WITCH HAZEL	30"-36" HT, 7 GAL	X
4	VIBURNUM ACERIFOLIUM	MAPLELEAF VIBURNUM	30"-36" HT, 7 GAL	X
5	VACCINIUM CORYMBOSUM	HIGHBUSH BLUEBERRY	30"-36" HT, 7 GAL	X
4	LINDERA BENZOIN	SPICEBUSH	24"-30" HT, 5 GAL	X
6	ILEX VERTICILLATA 'WINTER RED'	WINTER RED WINTERBERRY	24"-30" HT, 5 GAL	X
8	CORNUS SERICEA	RED TWIG DOGWOOD	24"-30" HT, 5 GAL	X
7	SAMBUCUS CANADENSIS	ELDERBERRY	24"-30" HT, 5 GAL	X
5	ARONIA ARBUTIFOLIA	RED CHOKEBERRY	24"-30" HT, 5 GAL	X
2	HYDRANGEA QUERCIFOLIA	OAKLEAF HYDRANGEA	30"-36" HT, 7 GAL	
10	RHODODENDRON 'PJM ELITE'	PJM RHODODENDRON	24"-30" HT, 5 GAL	
9	ILEX GLABRA 'COMPACTA'	COMPACT INKBERRY	24"-30" HT, 5 GAL	
3	SYRINGA VULGARIS	COMMON PURPLE LILAC	30"-36" HT, 7 GAL	
15	FORSYTHIA X INTERMEDIA	FORSYTHIA	24"-30" HT, 5 GAL	
8	PINUS MUGO	MUGO PINE	18"-24" HT, 7 GAL	
PERENNIALS				
57	PENNISETUM ALOPECUROIDES	DWARF FOUNTAIN GRASS	12"-18" HT, 2 GAL	
27	RUDBECKIA FULGIDA	BLACK EYED SUSAN	12"-18" HT, 2 GAL	
22	ECHINACEA PURPUREA	PURPLE CONEFLOWER	12"-18" HT, 2 GAL	

TREE PLANTING DETAIL

NOT TO SCALE



PROPOSED TREES:

NOT TO SCALE



OCTOBER GLORY



FREEMAN MAPLE



CLUMP RIVER BIRCH

NE WILDFLOWER SEED MIX SPECS

NORTHEAST NATIVE WILDFLOWER SEED MIXTURE PLANTED IN AREAS SHOWN ON LANDSCAPE PLAN

NORTHEAST WILDFLOWER SEED MIXTURE TO CONTAIN A MAJORITY OF > 67% PERENNIAL VARIETIES TO INSURE GROWTH OF WILDFLOWER VEGETATION IN SUCCESSIVE YEARS.

NEWP.COM/SEED-MIXES OR APPROVED EQUAL

SLOPE PLANTING TYPE	QTY	SIZE
Custom Blend of Northeast Native Wildflower Mixture	± 5,000 SF	SEED MIX



NE WILDFLOWER MIXTURE



WHITE CEDAR



CANADIAN HEMLOCK



STAR MAGNOLIA

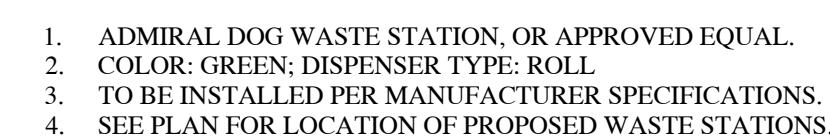
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Rev.	
Description	
No.	
ParkArchitecture.com 3 School House Lane, Suite #1 Elma NH 03750 Phone: 603-443-9400 Mobile: 603-727-9024 Email: parkarch@parkarch.com	
Client:	12 Tremont Street, LLC 77 Bank Street Lebanon, New Hampshire 603-727-9024
Sheet Title:	Landscape Details
Project Title:	Brookside Apartments Hartford, Windsor County, Vermont
PA Project #	2423
Checked By:	PS
Drawn By:	PS
Scale:	1"=20'
Date:	07-08-2024
L3.0	

NOT TO SCALE



1. SEE PLAN FOR LOCATION AND LENGTH OF PROPOSED FENCING. (BEGIN / END CLF LABELS.)
2. ALL CHAIN LINK FABRIC SHALL BE BLACK VINYL COATED.
3. ALL POSTS, BARS, RAILS & HARDWARE SHALL BE BLACK POWDER COATED STEEL FENCE.
4. CONTRACTOR SHALL INCLUDE TOP AND BOTTOM RAILS ON ALL FENCING.
5. MANUFACTURER SHALL BE ANCHOR FENCE OR APPROVED EQUAL.
6. INSTALL FABRIC WITH KNUCKLE SIDE UP AND POINTED END DOWN.
7. CONTRACTOR TO PROVIDE SHOP DRAWING TO LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO ORDERING MATERIALS AND INSTALLATION.

NOT TO SCALE



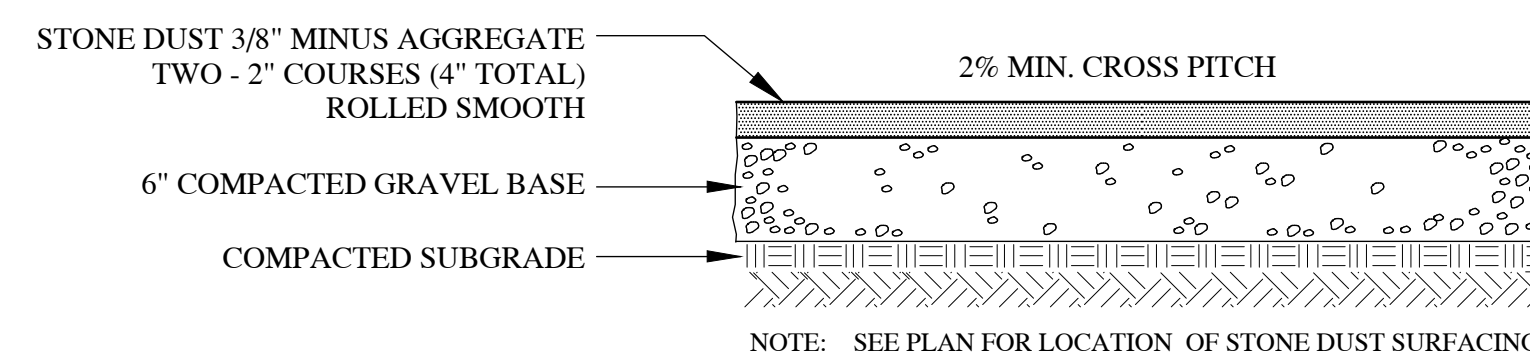
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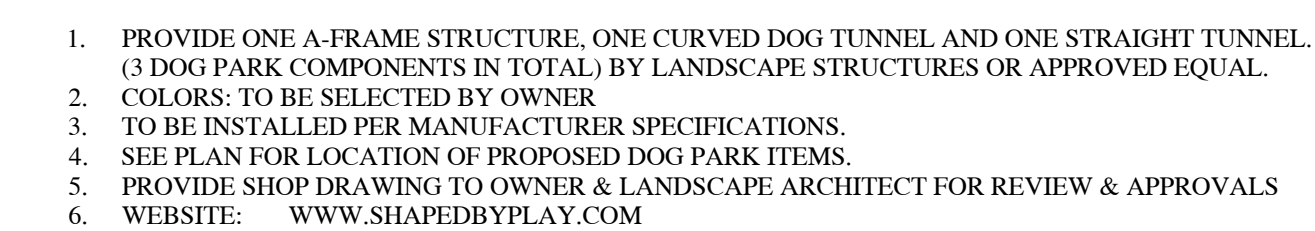
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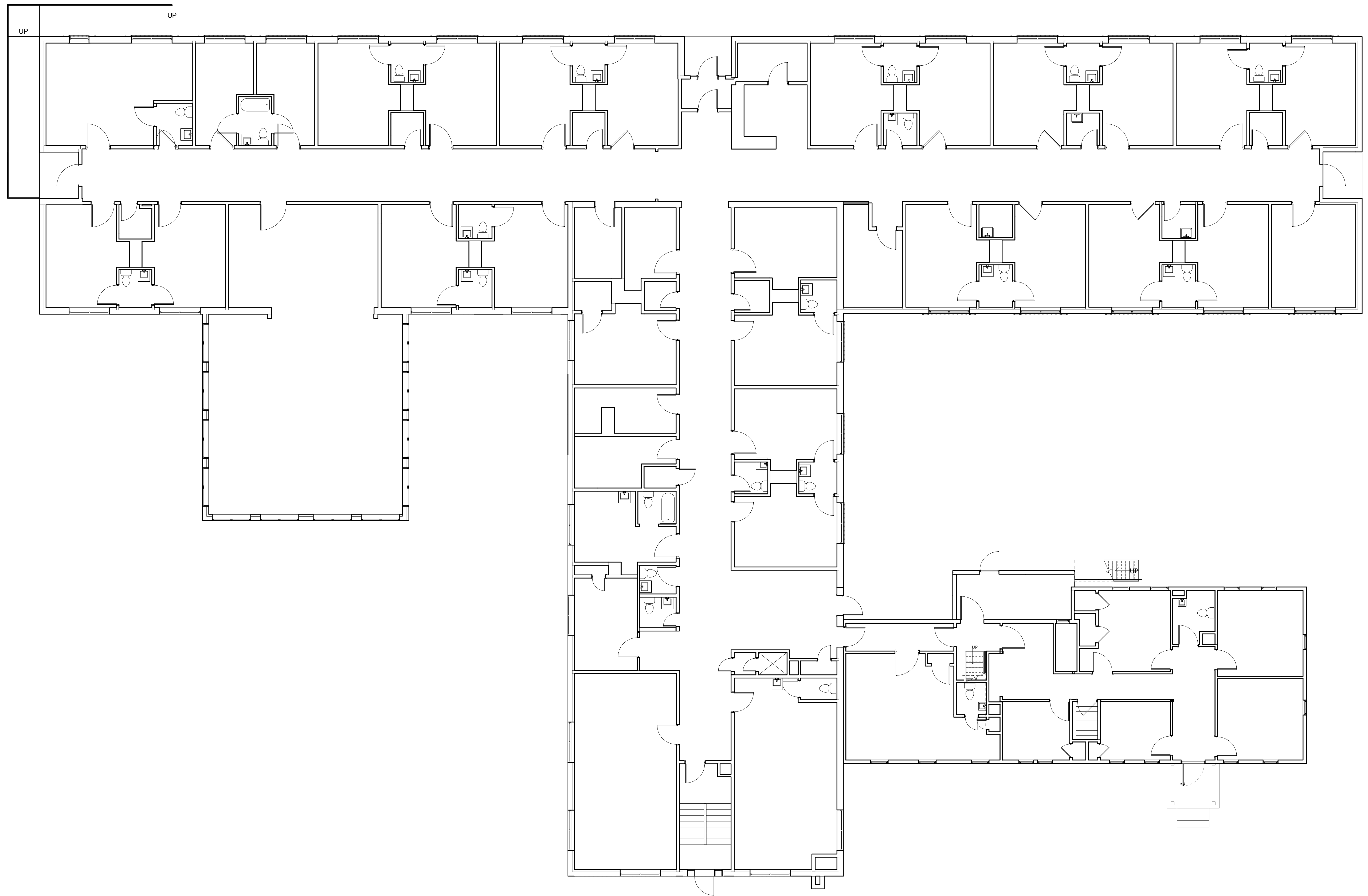
NOT TO SCALE



NOT TO SCALE



L3.2



STUDIO NEXUS
ARCHITECTS+PLANNERS
PO Box 275, White River Junction VT 05001
802 275 5110 | www.studionexusarch.com

12 Tremont Street, LLC Brookside Apartments



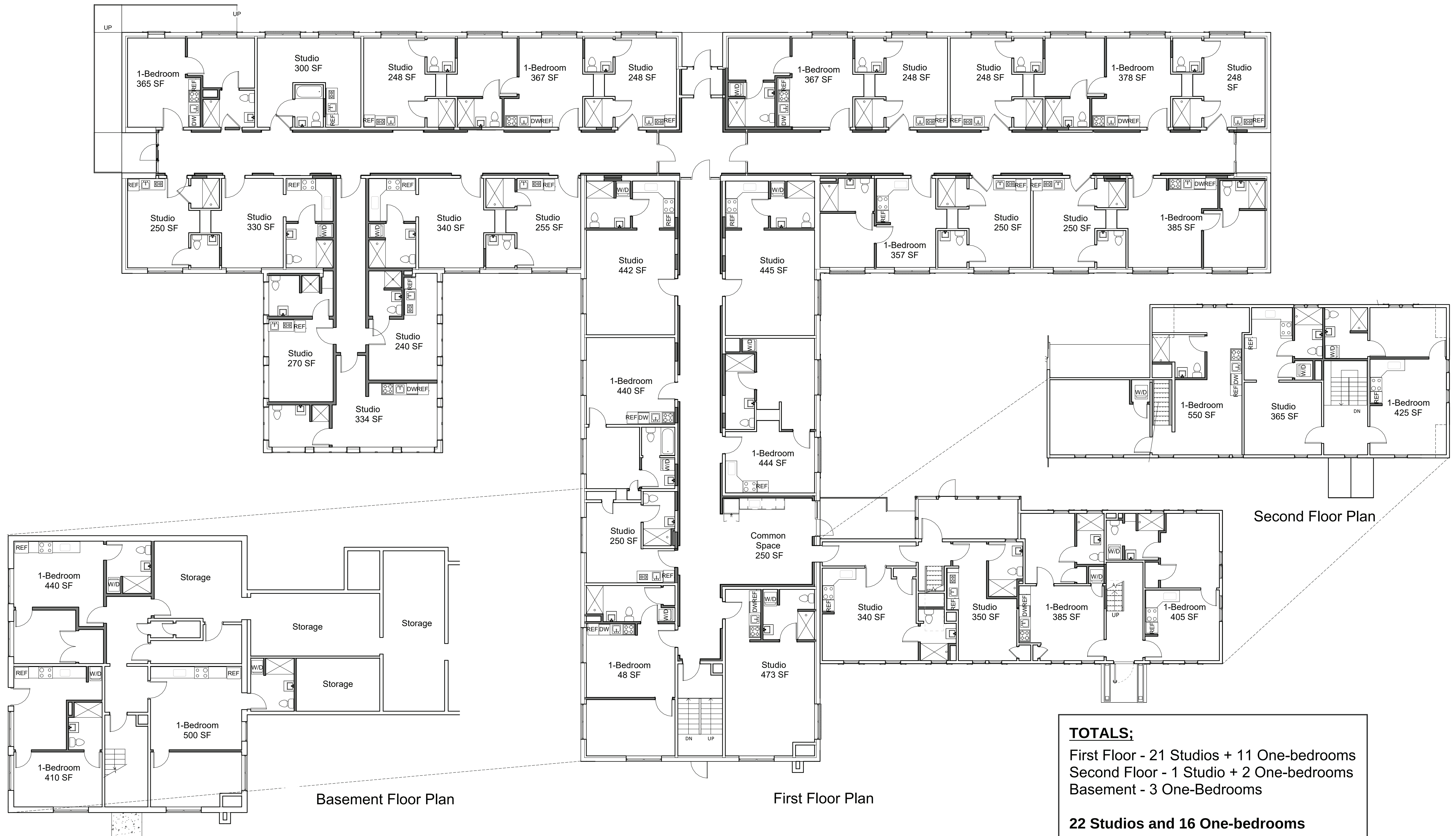
0' 4' 8'

Existing Floor Plan

SD1

June 24, 2024

1/8" = 1'-0"





View from Christian Street



View from Christian Street



View from Christian Street



Community Space

Memorandum

To: Tim Sidore
Cc: Jim Wasser, Adam Morse, Paul Simon, Tom Peltier, Dana Clawson, and Chris Holzwarth
From: Jo-Ann Ells
Date: July 2, 2024; **Response July 8, 2024**
Re: 1200 Christian Street- Multi-Unit Dwelling

Please submit a response to the following comments/questions on your application for a multi-unit dwelling before **9:00 a.m., Tuesday, July 9, 2024**. If you are unable to meet this deadline, please contact me as soon as possible to discuss the deadline to have your application considered at the next round of Public Hearings.

Prior to the Zoning Board and Planning Commission Hearings, you will receive an agenda, draft Findings of Fact, and additional staff comments if there are any. Please feel free to contact me at any time during this process.

1. Please confirm that you understand that you retain the obligation to identify, apply for, and obtain any relevant local and state permits for this project.
Understood.

To determine what State Environmental Permits may be required for your project, The Vermont Agency of Natural Resources (ANR) recommends that you use ANR's Permit Navigator Tool by going to the VT Department of Environmental Conservation website [Welcome to DEC | Department of Environmental Conservation \(vermont.gov\)](#) and going to the "Permit Navigator Tool" [Permit Navigator \(force.com\)](#)

If further assistance is needed, please contact the Community Assistance Specialist for our area:

Jeff McMahon, Community Assistance Specialist

Jeff.McMahon@Vermont.gov

802-477-2241

2. Please confirm you have contacted the Fire Marshal, Tom Peltier, to discuss obtaining a Public Building permit. tpeltier@hartford-vt.org (802-299-7013). **Confirmed.**
3. Any approval will include a condition that your engineer certify that no changes, other than those required by the Planning Commission/Zoning Board, were made to the final plans that are submitted for the Zoning Permit. Please confirm your understanding. **Understood.**
4. Any approval will include a condition that, in addition to the required hard copies, the final approved plans are emailed to the Zoning Administrator. Please confirm your understanding. **Understood.**
5. If you have not done so already, I encourage you to talk to Hartford's Environmental Sustainability Coordinator, Dana Clawson, about energy efficiency and new State rules pertaining to Commercial Building Energy Standards. (dclawson@hartford-vt.org) **Understood. We talked with Dana.**
6. Please confirm that your application complies with the new State rules regarding EV charging stations or amend the plans, as necessary. (Dana Clawson can assist you with this item if needed.) **Confirmed. Per Table C405.13.1 and equation 4-11 of the newest Vermont Commercial Energy Code we will be providing 40 EV Capable Spaces per the following requirement:**
 - **line diagrams on drawing C2.1 showing pathways for future Level 2 EVSE electric vehicle charging**
7. Preliminary approval of the Planned Development required that the Application for Final Approval includes:
 - Additional justification for a density bonus under section 260-47 C of the Zoning Regulations.
 - Applicant's Response: The density bonus as described in 260.47 (B-C) is appropriate given this project's merits. Specifically, single-occupant units are called for by 76% of the respondents in Hartford's 2019 survey. The Brookside redevelopment utilizes the existing facility while preserving the natural beauty of its surroundings, both on the property and in the neighborhood. The efficient use of resources, both

natural and constructed, promotes the harmonious use of property while preserving the existing topography, tree cover and geology.

Comment: Can you please provide information regarding the survey you are referring to? **Please see updated Narrative. The survey we are referring to is the 2020 survey conducted by American Community Survey as referenced by you below.**

The 2023 *draft* Housing Chapter (attached) identifies the need for smaller units. See Demographics and Housing on page 4 attached.

- A current survey meeting the requirements of the Subdivision and Zoning Regulations.

- Applicant's Response: Included in drawing set.

Comment: Please amend the survey to include the following signature block on the survey: **Amended and attached.**

This Plat is approved subject to the Findings of Fact, Conclusions of Law, and Decision for this subdivision, a copy of which is on file in the Hartford Department of Planning and Development Services.

Approval #_____ Hartford Planning Commission

Date _____ By _____
Hartford Planning Commission Chair/Vice Chair

- Information regarding the proximity of the proposed dumpster enclosure and dog park to the stream/wetland buffer.

- Applicant's Response: The dumpster pad will be made of concrete and is sloped so that the run-off runs away from the wetlands to a stabilized outfall. The dog park will have a pea stone surface and a 6" gravel base to filter and contain dog waste (see landscape detail on L3.2)

Comment: There is a stream on the property that is identified on Hartford's G.I.S. Natural Resources Map.

Section 260-25 B of the Zoning Regulations, Protection of Surface waters states:

A riparian buffer shall be maintained for a minimum of 100 feet from the top of the bank of the Connecticut, Ottauquechee and White Rivers, or for a minimum of 30 feet from the edge of a stream or any other surface water identified on the most recent edition of the Hartford G.I.S. Natural Resources Map. The riparian buffer also includes the area between the river and the top of the bank. No development, excavation, filling, clearing or grading shall occur within the buffer without conditional use approval from the Zoning Board of Adjustment except as specified in Subsections D and E.

It would be helpful if you could provide a plan that highlights the location of the riparian buffer as required by the Zoning Regulations and wetland buffer as required by the State and a narrative description of any activities proposed in the buffer areas. **Shown with a long, bold dashed lines on all the C1 and C2 sheets.**

In addition, please provide answers to sections 260-25 D, E, and F of the Zoning Regulations and information regarding any State permits required for activity in the wetland buffer.

260-25D: The dumpster pad encroaches by approximately 280 sf +/-, all other disturbed areas within the riparian buffer are part of existing parking areas to be maintained as parking and minor disturbances for stormwater management, a VT ANR wetland permit will be required for this and other work in the wetland buffer.

260-25E: The existing riparian buffer will be managed in accordance with Part E with the exception of the above dumpster enclosure and stormwater management encroachments.

260-25F: It is noted that a conditional use permit from the ZBA is required for the above outlined encroachments, the permit will be filed with the ZBA in accordance with 260-25-part F, section 1- and 2-. The location of the dumpster is the best possible location given the site constraints and proximity to existing parking areas. Other locations on the site would constrain emergency vehicle access and reduce parking. Disturbed areas will be replanted with dense native vegetation, steep slopes will be stabilized with riprap and or biodegradable erosion control netting and restored with native vegetation. The erosion prevention and sediment control plan will be followed during construction in accordance with the VTDEC EPSC Handbook.

- Updated information regarding the number of parking spaces/Information regarding visitor parking.
 - Applicant's Response: 46 parking spaces are proposed comprised of 38 parking spaces assigned to units and 8 visitor parking spaces.

Comment: Please confirm/correct if your justification for reduced parking is the same/has changed since Preliminary approval of the Planned Development. **The justification remains the same.**

- Confirmation that the applicant has contacted Advance Transit regarding providing a bus stop and the results of the discussion.

Applicant's Response: They were contacted by Tim Sidore several weeks ago (copy of email can be provided), however they have not yet responded

Comment: Suggested contact is Adams Carroll, Executive Director ACarroll@advancetransit.com 802 295-1824 ext. 202

Please provide an update. **Updates to follow.**

- Details regarding proposed indoor and outdoor amenities.
 - Applicant's Response: Site amenities at the redeveloped facility include the natural beauty of the site with its brook, a pond, generous lawn and thick woods, as well as a small courtyard with a gazebo and outdoor cooking facilities. Existing trees and shrubs will be selectively pruned; a wide variety of new trees and shrubs and 3,400 SF of wildflowers will be planted. We would like to add a small, fenced-in area at the south end of the property, near to the culvert, creating a dog friendly park.

The building itself will feature generous 8' wide, skylit hallways; near the main south entry a small community lounge space looks out onto the courtyard. The apartments, though small, will be well appointed with quality materials and fixtures. The walls, with their concrete block structure, minimize sound transmission and will keep the apartments quiet. Laundry facilities will be provided in some apartments and in publicly accessed laundry rooms. The high-ceiling

basement will provide additional tenant storage and depending on demand, could be repurposed to provide exercise and game room space.

No comment.

- An open space management plan including protection of the wetlands and stream.
 - Applicant's Response: We are in discussion with the Department of Environmental Services and will be applying for a wetlands permit if the project is approved, however for landscape of this type which includes wetlands, DES will want minimal management and the forest left wild.

Comment: Please provide an update. We agree in that DES will likely want minimal management of the forest here because of the nature of wetlands on this property. We have included riparian native VT species to add around the perimeter of the developed portion of the landscape as a measure to go back toward a natural environment within the wetland buffer areas. An additional column on the LA plan set was added to the plant list to indicate the riparian varieties.

- An Updated Master Plan:
 - Including the correct number of studio and one-bedroom apartments.

Applicant's Response: Done

No comment.

- Including the removal of "Zoning R-3" from item #2.

Applicant's Response: Done

No comment.

- Including setbacks for a corner lot.

Applicant's Response: Done.

No comment.

New comment- Please remove “Maximum lot coverage 65%” from the Master Plan. **Removed.**

- Updated Project Narrative

- Including correction of the gsf of the housing space in item #1

Applicant’s Response: Done

Comment: Please confirm the gsf is correct under item #1, Objective in the Master Plan for the dwelling units and common area and in the project narrative. **Confirmed at 19,500 GSF.**

- Removal of “Common Community Room” from item #2

Applicant’s Response: Done

No comment.

- Updated Plans including:

- A demolition plan

Applicant’s Response: Provided.

No comment.

- A landscape plan

Applicant’s Response: Provided.

No comment.

- Delineation of the stream, wetlands and their associated buffers.

Applicant’s Response: Delineated

Comment: See comments above regarding riparian and wetland buffers. This has now been reflagged by a wetland scientist and as stated earlier, native VT riparian species have been included in the buffer areas of the project.

- An updated photometric plan portraying the proposed site plan

Applicant's Response: Provided, although one fixture along Brookside Drive will need to be eliminated.

Comment: Please update the plan such that the proposed site plan is used as a base and remove the fixture you noted above. The plan has been updated and the fixture removed.

Please note that the proposed lights must conform with section 260-31 E of the Hartford Zoning Regulations which states:

Illumination from lighting fixtures or other light sources shall be shielded or of such low intensity as not to cause undue glare, reflected glare, sky glow, or a nuisance to traffic or abutting properties. Lights used to illuminate parking areas and drives shall be so arranged and designed as to deflect light downward and away from adjacent residential areas and public highways. Lights shall be of a "downshield luminaire" type where the light source is not visible from any public highway or from adjacent properties. Only fixtures which are shielded to not expose a light source, and which do not allow light to "flood" the property, are permitted to be attached to buildings. Searchlights are not permitted. **Noted. All fixtures will be dark-sky compatible.**

- A turning movement plan for Emergency Vehicles approved by the Fire Marshal.

Applicant's Response: Provided.

Comment: Please submit confirmation from the Fire Marshal verifying they have approved the plan. Please provide information regarding why you did not indicate that the fire truck would exit on to Brookside Drive. **A fire truck can exit onto Brookside Drive and we have added**

sheet C1.4 to illustrate this (a gate and knox box will be provided), however it also has the capability of performing a hammerhead turn and exiting back onto Christian Street. We have been in discussion with the Fire Marshal concerning our plans, and a final copy of the proposed has been submitted to him, but as of 7/8 no response has been received.

- A turning movement plan for passenger vehicles

Applicant's Response: This is similar to the garbage truck turning movement plan.

No comment.

- A parking space and a turning movement plan for delivery vehicles (i.e., UPS/Fed Ex)

Applicant's Response: Shown by dash lines at the primary entry. The turning movement is again similar to the garbage truck moving plan.

No comment.

- Stormwater Management plan

Applicant's Response: An erosion control plan and stormwater details have been provided in the civil drawings.

Comment: Please provide a narrative description of the stormwater plan.

A portion of stormwater runoff will be infiltrated into the ground and another portion will be treated in the grass swale with check dams along the south edge of the parking area. Along Brookside Drive in front of the building overall impervious surface has been reduced. Overall site impervious is expected to be increased by approximately 2,600 sf. Stormwater attenuation will be provided with underground stormwater detention chambers and infiltration. A curb will be provided along the south edge of pavement at the upper driveway off Christian St. to collect and treat runoff in the underground detention system. In addition, a portion of rooftop surfaces will also be collected in the same underground chamber system. Post development

stormwater runoff rates are expected to be reduced upon completion of the stormwater management plan.

- A note indicating that each dwelling units will have an assigned parking space

Applicant's Response: Noted on C2.0.

No comment.

- Identification of the main entrance to the building and lobby

Applicant's Response: Identified on C2.0.

No comment:

- Inclusion of areas for snow storage

Applicant's Response: Noted on C2.1.

Comment: Please clarify for all parking areas. Snow storage remains in the location to the northeast of the lower (southernmost) parking lot, as this location is away from the riparian buffer and is relatively level. Snow removed in other locations on the site will be trucked here.

- Applicable directional signage for vehicles

Applicant's Response: Shown on Civil set.

No comment.

- Parking spaces on Brookside Drive are limited to two accessible parking spaces as approved by the DPW.

Applicant's Response: In addition to the 2 handicapped spaces, which by code are required to be head-on we are providing 8 parallel parking spaces.

DPW Comment: The parking layout along Brookside Drive is

not acceptable to DPW as there are many conflicting turning movements and blind maneuvers required of motorists utilizing the accessible parking spaces.

The architect indicates that on street parallel handicap parking is not allowed. This is true where an aisle cannot be provided, and the handicap person would be entering and exiting within the roadway. There appears to be adequate room to establish the necessary improvements to allow parallel parking for handicap individuals. As a note, exceptions are allowed for historic sites with space limitations such as White River Junction and Hartford Village.

Please contact the DPW and amend the plans (and any applicable application materials) such that they are approved by the DPW and comply with the Zoning Regulations. Chris Holzwarth was contacted. Our plan for parking along Brookside (north of the building) now conforms to the sketch be provided for us with 6 parallel parking spaces, two of which are accessible. The additional 4 spots (from what was approved for the PUD) are necessary to provide some additional accommodation for folks with limited mobility (the southern entry is *not* accessible) and because the bulk of the housing units (21) are located in the northern portion of the building. At the southernmost parking lot we added 4 additional parallel parking spaces to maintain our total of 46. We did not provide head-on parking at the southern lot per his sketch as it would require a wider access road and a retaining wall into the hill.

- Identification of all outdoor and indoor amenities

Applicant's Response: Identified on drawings.

Comment: Please review the civil, landscape and architectural plans for consistency and update, as necessary. The landscape plans are provided and demonstrate a change in what was provided between the preliminary submission and the final permit plan set. The outdoor amenities include several outdoor gathering areas, a gazebo, outdoor dining with a BBQ station, multi-purpose open space area, a nature

walk (stonedust walk) along the forest perimeter, and an enclosed dog run area with equipment.

8. Please update the number of proposed parking spaces on the application form.

Updated.

9. Please update the project narrative to reflect the density calculation as described in Findings 14 & 15 of the preliminary approval of the Planning Development.

Updated.

10. I have an email stating that the gate is no longer proposed, however, it is shown on sheet C2.1. This gate is referred to in Findings #13 and #18 of the Preliminary approval of the Planned Development. That email was a mistake and the gate and a knox box will be installed per C2.1, note C1 and approved by Tom Peltier.

Please clarify and amend the plans, as necessary.

11. Please respond to the following concerns of the DPW.

The 10" sewer line at the projects entrance was developed before the unpermitted/lower parking lot was established. The main only has 4' of cover which is fine in cross country and unpaved situations. The line should be inspected and rated for the necessary structural cover and insulation requirements. The engineer should provide an acceptable plan to provide some combination of moving parking spaces, providing cover, insulation, or replacement of pipe to conform to the town and state standards.

We have specified 4 inches of XPS insulation over the existing sewer line within the paved parking area. The insulation would be placed at the bottom of the parking lot engineered fill section. The added insulation will bring the sewer line into compliance with Section 1-1002(g) 2(B) of the VT Wastewater System and Potable Water Supply Rules.

12. Please submit additional details regarding how you calculated your anticipated traffic volume and the traffic volume for the former nursing home. Details have been provided (and updated) in the narrative utilizing the most recent ITE Trip Generation Report (8th Edition) calculator.

13. Please provide a description of how the site plan provides for the safety and convenience of pedestrians, bicyclists, and handicapped persons including lighting of walkways and entrances, design and placement of walks and crosswalks, pick-up points for public transportation if offered, and bike racks. (Please consider providing additional bike racks.) We are providing 3 accessible parking spaces (2 by the accessible entrance on the north side and 1 for the units at the basement level) directly adjacent to the entries they serve. A nearly continuous walk is provided around the north, west and east sides of the building, continuing into the courtyard/gazebo/BBQ/deck area. In addition, a 5'; wide accessible pathway leads to the southernmost parking. A stonedust path leads to and around the dog park, the open and wooded areas to its south and the southeasternmost tip of the parking lot. As shown on the photometric plan, all seven building entries are well lit, including the primary entry on the south, and the courtyard on the east. Adequate but neighborhood sensitive lighting has been provided for the parking and drive aisles. There is parking for 6 bikes located adjacent to the north entry.
14. What will be done to ensure that the landscaping is maintained in a healthy condition? The project plans include a 2-year guarantee on all proposed landscape plantings. In addition, all varieties specified are VT native and hardy varieties that will thrive on their own once established. The owner has an in-house maintenance team that will provide regular landscape maintenance including mowing lawn areas, pruning, mulching beds annually, etc.
15. Please outline how the application conforms with section 260-27 C of the Hartford Zoning Regulations which states: Adam

C. Grading and drainage. The following provisions shall apply to any changes which affect drainage of water to or from a property:

- (1) Any changes in grading shall be made so that runoff is directed to established drainage courses and will not cause ponding or flooding of other properties, or exceed the capacity of downstream drainage facilities.
- (2) When possible, drainage systems shall be designed so that water runoff existing prior to site development shall not be increased after development.
- (3) All changes in grade shall be controlled so as not to cause a nuisance or damage to other properties or erosion of topsoil.

Pursuant to section 260-27, part C of the Town of Hartford Zoning Regulations, the site grading has been designed so that runoff is directed to established drainage swales and curbing and will not cause ponding or flooding of other properties or exceed the capacity of downstream drainage facilities. The

drainage system will be designed so that water runoff existing prior to site development will not be increased after development. All changes in grade will be controlled so as not to cause a nuisance or damage to other properties or erosion of topsoil.

16. Please acknowledge that during construction erosion and sediment control measures as specified in the most current edition of the *Low Risk Site Handbook for Erosion Prevention and Sediment Control* published by the Vermont Department of Environmental Conservation will be followed. **Acknowledged.**
17. Any approval will include a requirement that no structure, including construction/office trailers, signage, parking spaces or plantings may be placed in such a way that obstructs sight distance along the traveled way, at any intersection or driveway. Any signs, parking spaces or plantings found to obstruct sight distance will be required to be relocated. Please confirm that you understand this requirement and that all proposed structures and plantings meet this requirement or amend the plan accordingly. **Understood.**
18. Please acknowledge that you understand that once the project is complete and before I can issue a Certificate of Occupancy you need to:
 - Submit written verification from your Professional Engineer certifying that the project was constructed in compliance with the approved plans and Findings of Fact
 - Submit written verification from your Architect certifying that the project was constructed in compliance with the approved architectural plans and Findings of Fact.
 - Submit written verification from your Landscape Architect certifying that the project was constructed in compliance with the approved architectural plans and Findings of Fact.
 - Submit verification that the water and sewer have been inspected and approved by the Department of Public Works.
 - Submit a record drawing to the Department of Public Works with swing ties for the water and wastewater lines.
 - Submit a PDF of the record drawing to me.
 - Record an Energy Code Certificate in the Land Records.

Please acknowledge that you understand these requirements. **Acknowledged.**

19. Please acknowledge that you understand that construction of the project must comply with Section 260-31 B of the Hartford Zoning Regulations, which states:

Earth moving and hauling, rock drilling or crushing, jack hammer and similar excessively loud equipment shall not be operated on Sundays or between the hours of 5:30 p.m. and 7:00 a.m. except in emergency situations. **Understood.**

20. Please acknowledge that you understand that any stockpiling of materials related to the construction of the project must be located twice the distance of the height of the stockpiled materials from any public or private right-of-way or property boundary. **Understood.**

21. The Hartford Zoning Regulations provide that “Construction trailers may be used at construction sites for office and laboratory use on the condition that they are placed a minimum of ten feet from any property line, are used for a project with a valid zoning permit and are removed before a certificate of occupancy is requested. No construction trailer may be placed in such a way that obstructs sight distance along the traveled way, at any intersection or driveway. Any construction trailer found to obstruct sight distance will be required to be relocated immediately.”

Please acknowledge that you understand these requirements. **Understood.**

22. The project requires 3 approvals:

- Final Approval from the Planning Commission for the Planned Development
- Site Plan Approval from the Planning Commission
- Conditional Use Approval from the Zoning Board of Adjustment

The materials for Site Plan and Conditional Use approval are the same.

23. Please send me two separate emails for these applications including:

Planned Development: Application form, Master Plan, Narrative, response to this memo, survey, and the revised plan set.

Site Plan/Conditional Use Approval: Application form, Narrative, response to this memo and the revised plan set.

24. Please submit:

Eight (8) hard copies of the application materials listed above for the Planned Development. Please fold the plans.

Fourteen (14) hard copies of the application materials listed above for Site Plan/Conditional Use Approval. Please fold the plans.

(The plans submitted were a little difficult to read. Please consider providing larger plans)

143 Brookside Drive

White River Junction, VT 05001-9259

July 8, 2024

Re: Proposed Brookside Apartments, 1200 Christian Street, Hartford, VT

Dear Hartford Zoning and Planning Commissions,

As the south abutter of the proposed Brookside Apartments on 1200 Christian Street, we believe this project could well serve our community.

Having built our house 40 years ago, we have seen changes in our little neighborhood- the development of Apple Lane off of Brookside Drive, the addition of apartments on Brookside Drive and Christian Street, and the closure of the Brookside Nursing Home which directly affected the housing and care of a parent. Change is often unsettling.

However, we have carefully considered and studied the Brookside Apartment proposal details and find that we are in support of this project. Housing is a major barrier to the local economy. This repurposing/recycling of the Brookside Nursing Home into apartments can provide the opportunity for hard-working people to have a place to live closer to where they work.

Sincerely,

Michael & LeeAnne Heeremans

July 11, 2024

Dear Hartford Zoning Board and Planning Commission,

We are representing a group of 18 property owners who live next to, and near the proposed development of the former Brookside Nursing Home at 1200 Christian Street. We are submitting this letter to be part of the pre-meeting materials to be reviewed by the upcoming July Zoning Board and Planning Commission meetings where Final Approval of the Brookside Nursing Home Development Project is being sought.

We have appreciated the improvements that the Developer has made to the building and grounds to date. There are, however, a number of serious concerns that remain regarding this project, as summarized below:

1. We are asking for a formal presentation to be done by the Developer at the July Zoning and Planning Commission meetings when the Final Approval application is being reviewed.
2. The Brookside project land is mostly surrounded by single family homes. The Developers refer to apartments nearby to suggest their plan “fits” the neighborhood, however these were pre-planned apartment complexes which were proposed as fully-equipped apartments and built for that purpose. The Developer also makes several comparisons to “in-town/downtown” buildings they have developed in Lebanon and White River Junction. These, however, are not appropriate comparisons since the location of this property is not in the downtown area.

Given the above, we are requesting that both the Zoning and Planning boards each do a site visit/review to actually see “the lay of the land” – the majority of which, again, is surrounded by single family homes. We ask that all abutters and neighbors to the Brookside property are invited to join that review.

3. a) The extremely small size of the proposed units is of great concern. Eighteen of the units are Studio apartments with a size of 250-350 sq ft, and 7 of the 1-bedroom apartments are less than 385 sq ft in size. In total, 25 of the 38 units are less than 385 sq ft. These units do not provide enough space to include a stove/oven, washer/dryer, closets and dressers, and places to sleep, while housing 2 adults, a child under 2 years old, and a pet dog. **We would like to note that the US Housing and Urban Development lists these size ranges for public housing: Studio: 500 sq ft, 1 bedroom 700 sq ft, 2 bedroom 900 sq feet (HUD.gov), sizes well above the current proposal.**
 - b) Clearly the Developer would want to maximize each square foot of building for occupancy, but the proposed density is not ideal for either the residents or the broader

community. When such small apartments are in areas with nearby amenities (laundry, transportation, stores, restaurants, public parks), it may make it easier for people to have a reasonable quality of life with limited space in individual apartments. **But that is not the case with this site; it actually feels like a proposal for high density urban apartments in what is basically rural/suburban area.**

4. Since it seems unlikely there is room for washer/dryer in the studios, and we do not see a laundry room on the final project proposal, it appears residents will have to travel to a local laundromat (the closest one is in a building on Hartford Ave, currently 1 mile away and is currently for sale).
5. We, the abutters, and neighbors, have been very clear to the Developer and the Planning Commission that we feel that the density of this project, at 38 units, is too high. There seems to be a rush to squeeze in as many units as possible. It is also important to note that the Planning and Zoning commissions just approved the Sykes avenue project which will be adding 250 rental units to the Town of Hartford. Trying to squeeze 10 more 250 sq ft studio apartments into this property, and project, does not fit the existing building and neighborhood.
6. To get to 38 units, the Developer is applying for a density bonus. Unless there is a mechanism to ensure that affordability requirements are maintained, what is the guarantee, that if granted by the town, these additional units will serve the intended purpose of increasing affordable housing for the required term?

We suggest that if a density bonus is granted, that it be conditional upon annual town/third party compliance monitoring, paid for by the developer in accordance with a compliance monitoring plan/covenant to be approved by the town, reviewed by town legal counsel, and agreed to by the developer.

7. The present plan of 38 units, some housing more than one person, will more than double the total number of people who are living in the neighborhood, that includes the new 1200 Christian Street residents and existing abutters and neighbors on Christian Street, Brookside Drive, and Apple Lane. We would expect an increase in Hartford police presence.
8. **We are extremely concerned that there has been no required traffic study by Town.** The traffic on Christian street has increased greatly since the nursing home was originally built. The Developer's comments comparing traffic flow between the original nursing home and this development was called "absurd" by one neighbor.

Rarely did nursing home residents travel when residing at Brookside Nursing Home, and few, unfortunately, had visitors. Staff who worked at Brookside commuted during shifts

across their 24-hour operating hours. Comparing the 66 nursing home residents living at this property, vs. 38 dwellings is an inappropriate comparison, as some of the new studio and one-bedroom units will house more than one resident.

The Developer is significantly underestimating the traffic involved in the future apartment dwellers commuting, their outside visitors, deliveries of meals, FedEx, UPS, US Mail coming in and out of the parking area.

9. Individuals without a vehicle will have difficulty living in this development.
 - a) **The closest Advanced Transit/AT stop is one mile way.** We see that in the final proposal there have been unsuccessful attempts by the Developer to contact Advanced Transit. Without a commitment of Advanced Transit to have Brookside Drive/1200 Christian Street as part of their route, it will make commuting very difficult for renters without a working vehicle. According to Google Maps it is a 20-minute walk from Brookside Drive, along Christian and Bugbee streets, walking on the side of the road with no sidewalks, making this especially challenging and dangerous, especially in winter.
 - b) The idea of a shared car available is good in theory...but who will manage that? Does the Developer have experience in their other properties offering this type of service? It seems likely that residents without a vehicle may well feel “trapped.” How will the Town hold the Developer accountable to provide and maintain this auto service?
 - c) There is reference to a bicycle rack in the final proposal. Traffic, weather, and lack of bike lanes on Christian Street and Bugbee make bicycle riding as a form of transportation very unsafe, and unlikely.
10. Reference has been made to a gazebo onsite as a gathering spot for residents. The existing open air wooden structure is only usable when weather allows. There are **no indoor spaces** in the final plans designated for resident gatherings to allow residents of the 38 units to connect with each other.
11. We see a commitment to landscape design in the final proposal. Will the proposed parking lot provide landscaping of trees/shrubs that will help screen it from Brookside Drive and Christian Street views--- with the exception of clear eyesight for entering/exiting the complex from Christian Street?
12. What is the policy regarding residents and visitors who smoke? Will smoking be allowed in the apartments, or in designated smoking areas on the grounds?

13. We believe the Developer has overstated their efforts at preservation of the natural setting of the brook, trees etc. We continue to have questions regarding this:

- a) Does the 50% “open space” include the paved parking lot?
- b) Will the mature black locust trees opposite 1187 Christian Street be preserved?
- c) The Developer refers to “a pond” but is that actually something that exists on the current property? Or does the ecosystem have to be altered to create that?
- d) How would a “dog park” enhance that aspect of the property, and is that really needed? It is likely to actually increase traffic to and from the development. In addition, the Town of Hartford’s Watson Dog Park, and Kilowatt Field are within a very close distance.

We do not understand why the Planning Commission ignored our previous concerns regarding the impact on the natural resources on this site. We need to note, again, that the State of Vermont has identified Brookside creek as a nesting area for deer. The Town of Hartford Natural Resource map talks about protection of surface water and wildlife habitat. We see deer frequently emerge from the woods and drink from the stream that runs through the 1200 Christian Street property. We have also observed countless other animals using the wooded property onsite, including birds, woodchucks, rabbits, etc. We strongly recommend that the natural habitat, in particular the stream and wooded areas, not be disturbed by the new development.

We ask that the Hartford Conservation Commission be consulted on this project regarding how to preserve the integrity of the natural resources of this property.

14. It was good to see more specifics regarding parking lot and outdoor lighting in the final proposal. It is key to limit the negative impact of the project on the existing neighbors, most of whom are in single family homes on Brookside/Christian Street. We strongly support the careful selection, review and adjustment of the lights themselves, and landscaping that helps keep lighting for parking and not shining into the abutters and neighbors’ homes.

15. We want to again confirm there is a solid commitment by the Developer, and Town, to lock the Brookside Drive entrance and that this will be rigorously enforced. Our preference is that it be closed completely.

- a) The Rogers and Klymn families, abutters whose home driveways are directly across from that are especially concerned that this entrance be rarely (if ever) used.
- b) As part of the previously requested traffic study, we also want to note that the road on the north bend of Brookside Drive next to the nursing home is eroding which is a dangerous situation. It is also a blind corner, flanked by guard rails, which creates

- significant issues for emergency vehicles including fire trucks and ambulance. The guard rails have been severely damaged several times by large vehicles failing to make the turn slowly and safely. This has required the Town of Hartford Public Works team to repeatedly come out and do extensive repairs.
- c) One suggestion is to name the Development “Christian Street Apartments” to support the above.
- 16.** What is the existing track record of the Developer regarding keeping up their other properties, and what is the longer-term commitment regarding that for the Brookside project?
- 17. Neighbors attending the 5.20.24 Planning Commission meeting believe the Planning Commission moved too quickly to grant preliminarily approval for this project, despite the significant r54 concerns expressed to the Planning Commission in writing before, and verbally during the meeting.**

We witnessed a great deal of interest/support for the Developer and their wants/needs, but very little acknowledgement of our previous letter to the Commission at that meeting – which included input from many of the neighbors and abutters to this project. It is of utmost importance that both Zoning and Planning Boards are careful in their review and decision making about this project.

We look forward to your serious consideration of these important concerns regarding this Project, and the discussion of them at the upcoming July Zoning and Planning Commission meetings.

Respectfully,

Jesse Klymn, 246 A Brookside Drive
Anika Gilbert, 246 B Brookside Drive

Written with input and strong support from these 17 Hartford property owners, all abutters, and neighbors to this project:

Frank Klymn, 246 Brookside Drive
Jessica and Jay McDonough, 50 Brookside Drive
Carey O'Neil, John O'Neil, 88 Brookside Drive
Emily and Dillon Owens, 110 Brookside Drive
Benjamin Davis, 136 Brookside Drive
Susan Goodwin, 196 Brookside Drive
Deborah Rodgers, 256 Brookside Drive
Jeff Arnold, 1187 Christian Street
Hilde Ojibway, 1285 Christian Street
Michael and Dorothea Barton, 35 Apple Lane

Jenny Horn, 36 Apple Lane
Jeni and Jeff Frechette, 50 Apple Lane
Mark and Kathy Detzer, 59 Apple Lane
Marie Wesson, 80 Apple Lane
Ann Kerrigan, 98 Apple Lane
Susan Loskutoff, 143 Apple Lane
John and Kathy Wetzel, 157 Apple Lane